



POQUONNOCK BRIDGE VILLAGE

DESIGN GUIDELINES

Second Public Presentation

April 11, 2019



Project Team:

Town of Groton, Planning & Development:

Jon Reiner, AICP, Director

Deb Jones, Assistant Director

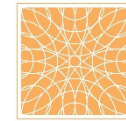
Diane Glemboski, Planner II - Land Use



Union Studio Architecture and Community Design:

Don Powers, AIA, LEED-AP, CNU, Founding Principal

Jeremy Lake, AIA, LEED-AP, CNU, Senior Associate



UNION STUDIO
ARCHITECTURE & COMMUNITY DESIGN

Horsley Witten Group:

Nathan Kelly, AICP, Associate Principal


Jeff Davis, AICP, Project Planner



Agenda:

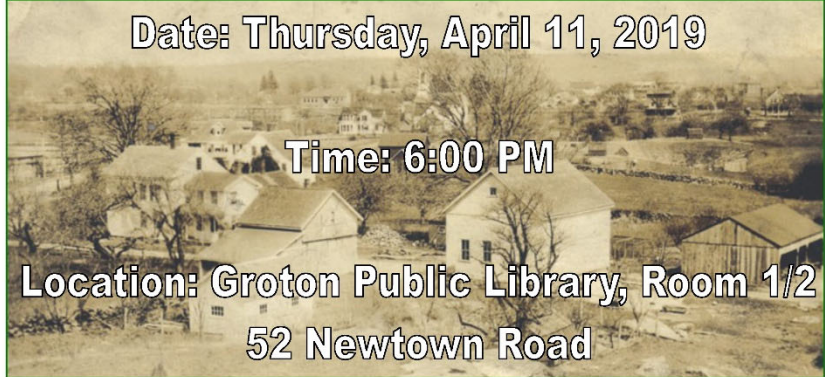
- Team Introductions
- Process/Schedule
- Recap of First Presentation
 - Project Location
 - Project Background
 - Draft Design District
- Visual Preference Survey
- Zoning Regulations Update
- Draft Design Guidelines
- Illustrative Examples
- Discussion

AGENDA



Poquonnock Bridge Village

Public Meeting



Date: Thursday, April 11, 2019


Time: 6:00 PM

Location: Groton Public Library, Room 1/2
52 Newtown Road

What's Going On?

Groton's Plan of Conservation and Development (POCD) envisions the Poquonnock Bridge area as a mixed-use village with development that enhances the historic sense of place and promotes pedestrian-friendly redevelopment. Come hear a presentation of the draft guidelines for new development in the village. We'll review the boundaries and talk about how the guidelines work with the proposed mixed use zone for the area. We'll also present some sketches to show how the guidelines could work on three properties in the village and talk about the Route 1 streetscape.

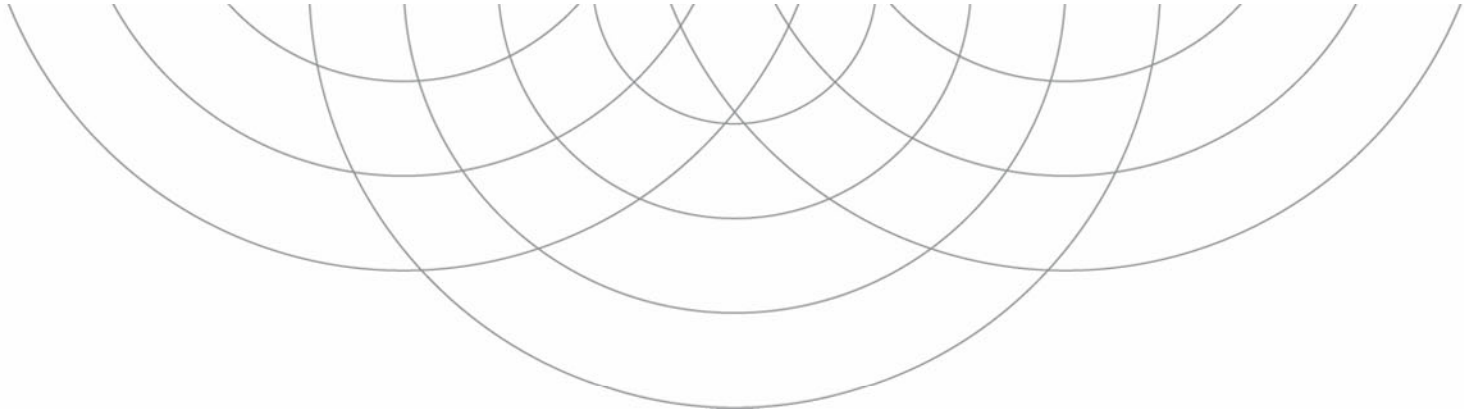
*Come help guide the town as we re-establish
Poquonnock Bridge Village as a special place*



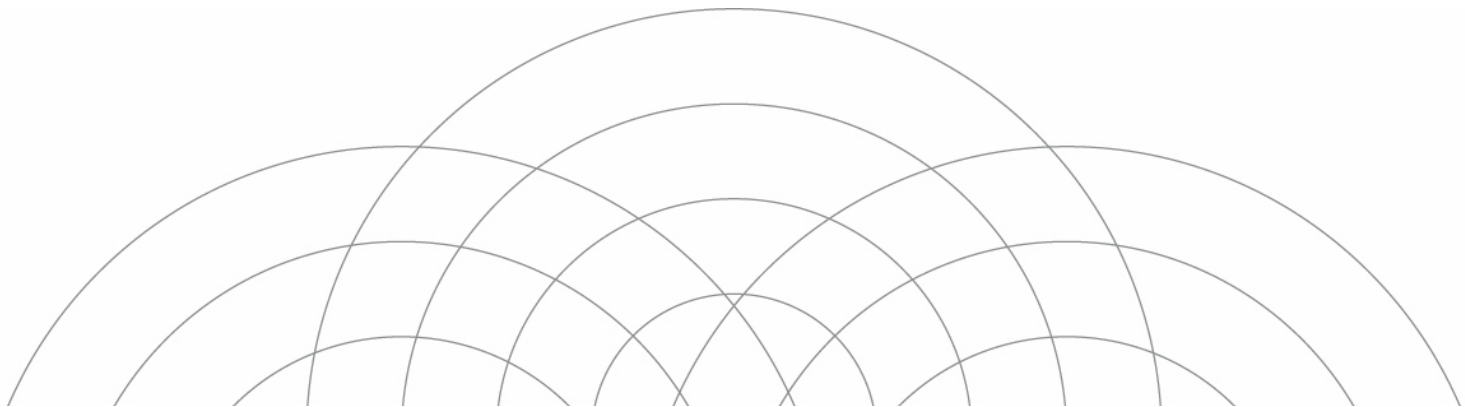
For more information about this project, please contact:

Deb Jones, Office of Planning & Development Services
Town of Groton
(860) 446-5970 or djones@groton-ct.gov

POQUONNOCK BRIDGE VILLAGE



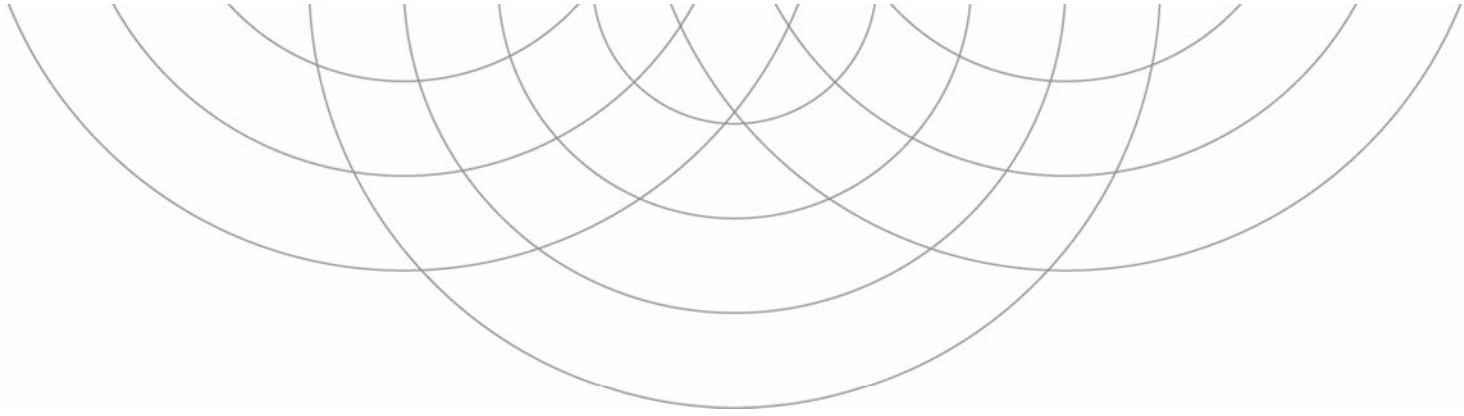
Process/Schedule



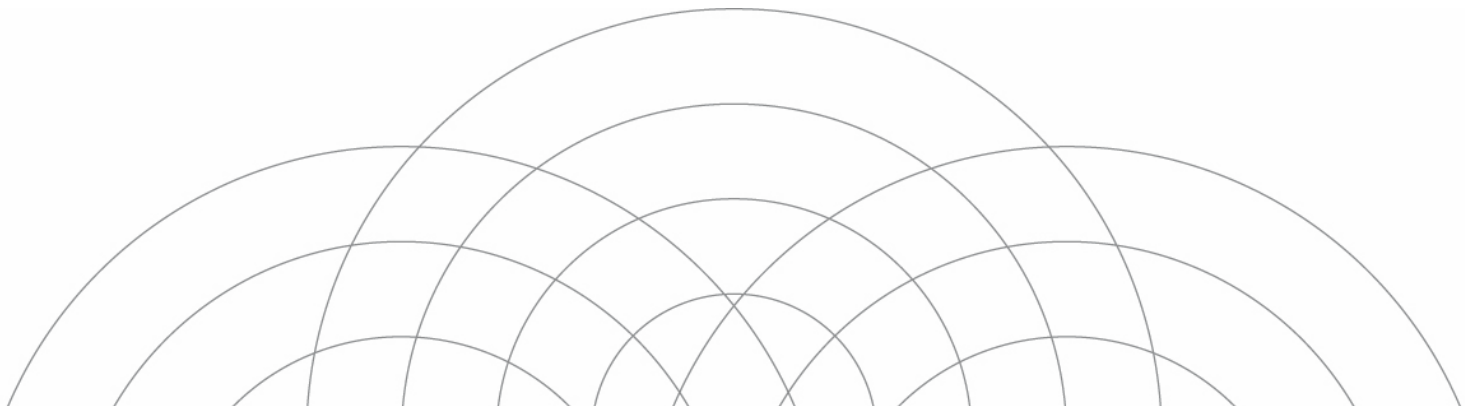
Task 1: Pre-Design	November – December 2017
Task 2: Inventory of Existing Conditions	January – March 2018
Task 3: Design District Boundary	Ongoing
<i>Task 4: First Public Presentation</i>	<i>April 30, 2018</i>

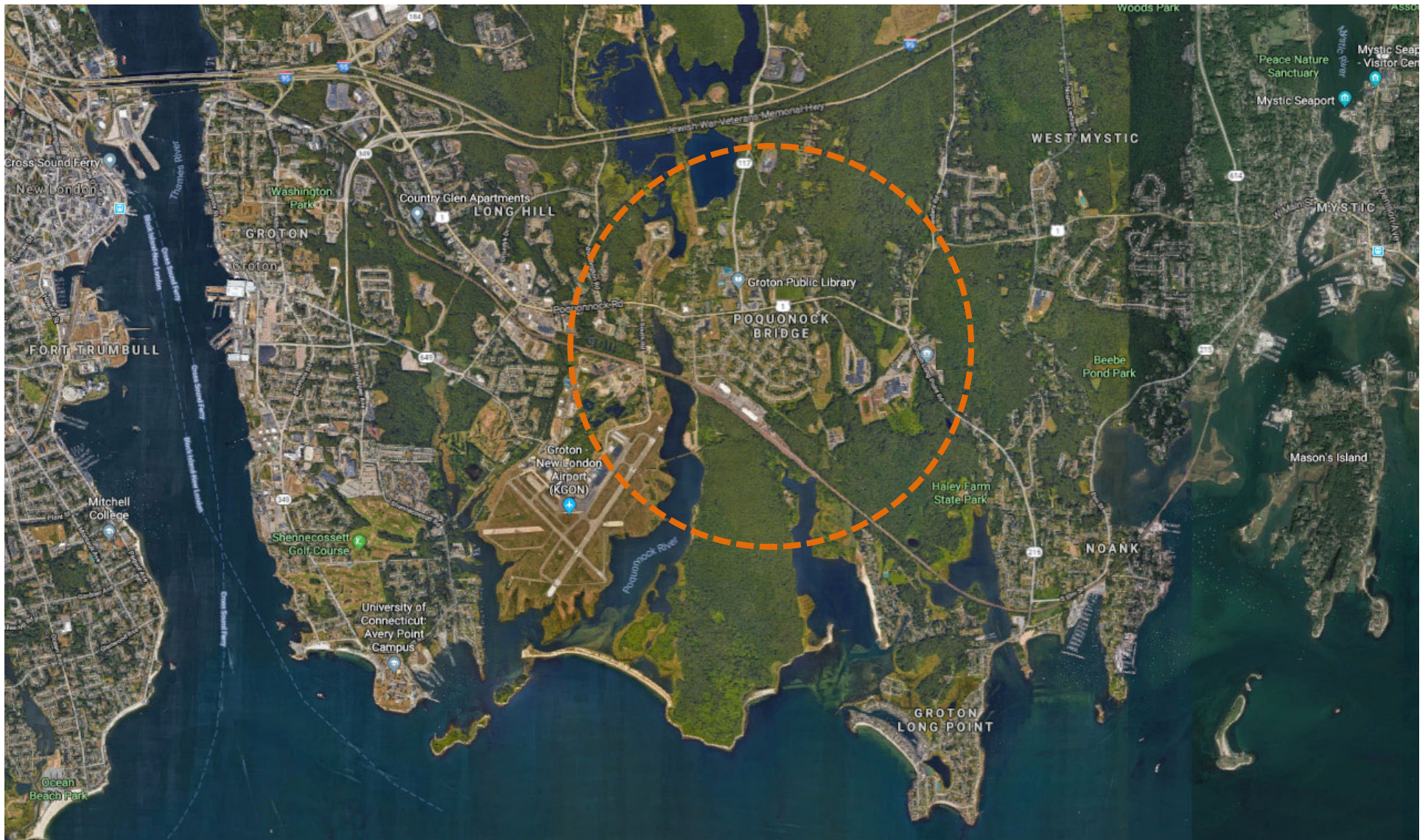
(Hiatus for rezoning efforts)

Task 5: Analysis of Inventory and Conceptual Diagrams	November 2018 – January 2019
Task 6: Draft Design Guidelines	February – March 2019
<i>Task 7: Second Public Presentation</i>	<i>Today, April 11, 2019</i>
Task 8: Final Design Guidelines	April – May 2019



Recap of First Public Presentation:
Project Location





PROJECT LOCATION

POQUONNOCK BRIDGE VILLAGE



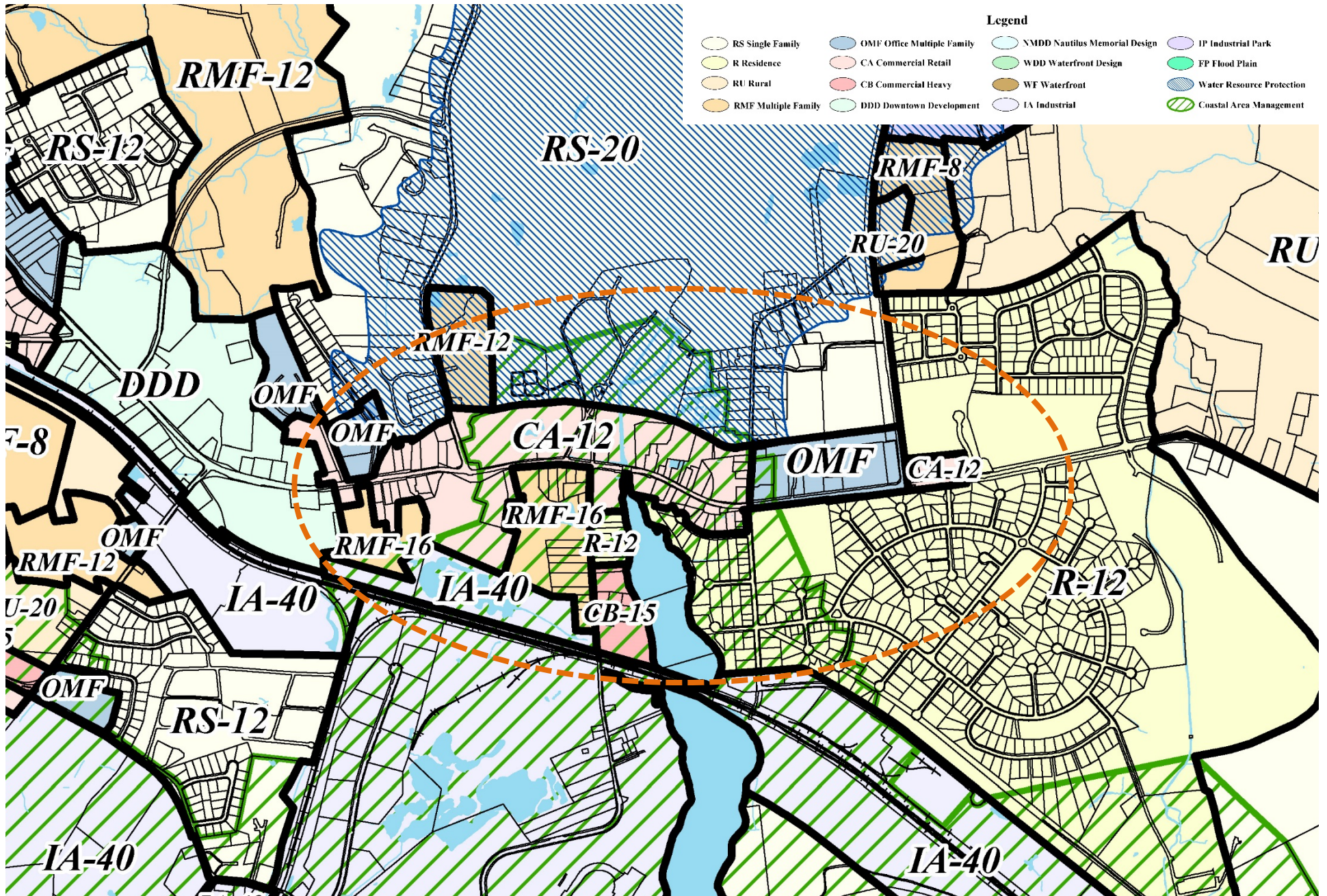
PROJECT LOCATION

POQUONNOCK BRIDGE VILLAGE



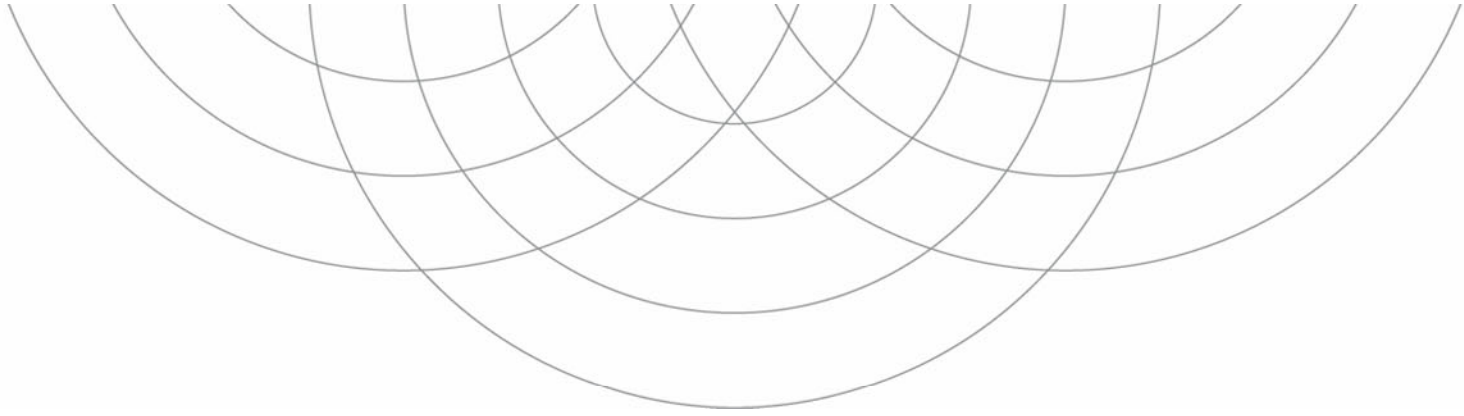
PROJECT LOCATION

POQUONNOK BRIDGE VILLAGE

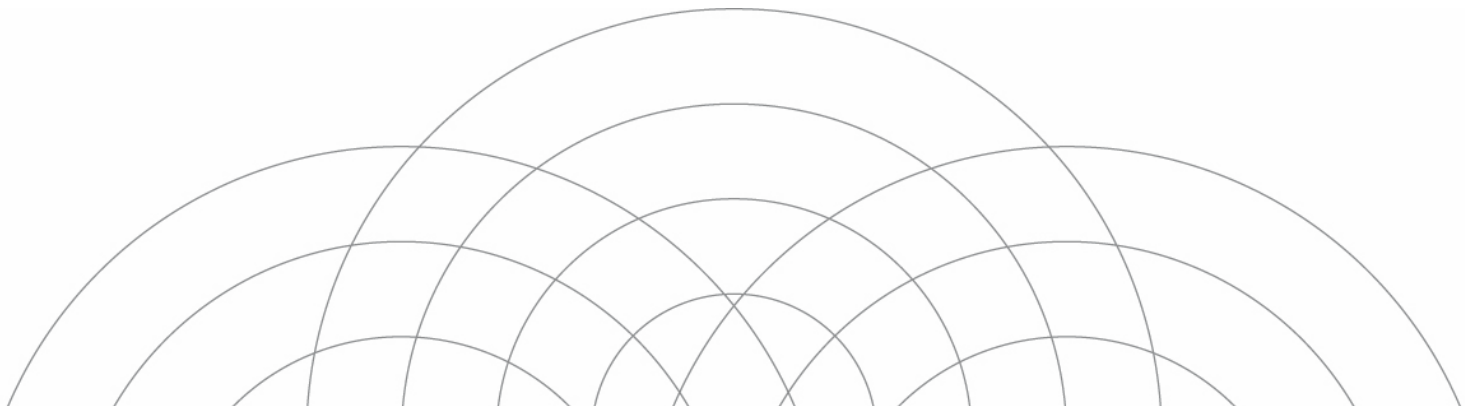


CURRENT ZONING

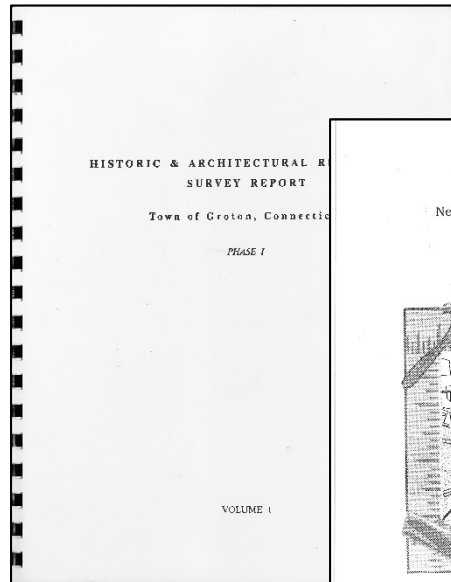
POQUONNOCK BRIDGE VILLAGE



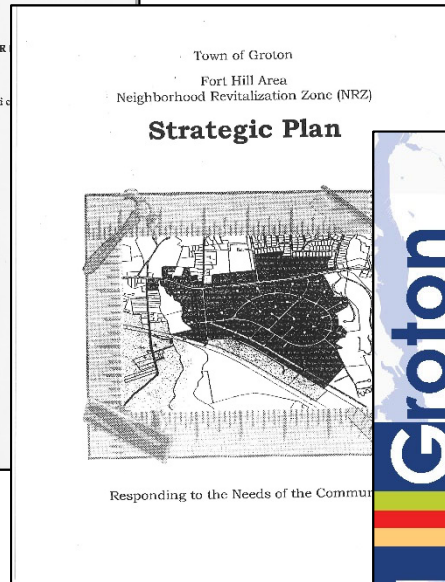
Recap of First Public Presentation:
Project Background



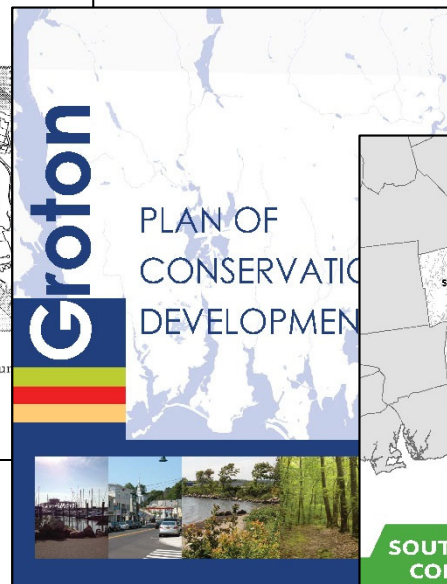
Background Materials:



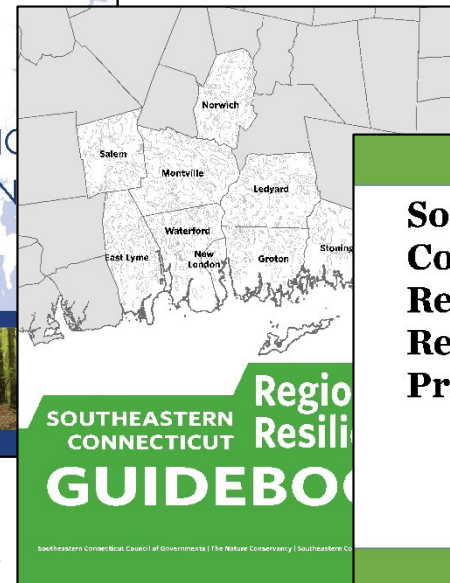
Historic & Architectural Resources Survey Report, Town of Groton, CT, Phase I – Volumes 1-4, May 1992



Fort Hill Area Neighborhood Revitalization Zone (NRZ) Strategic Plan, March 2000

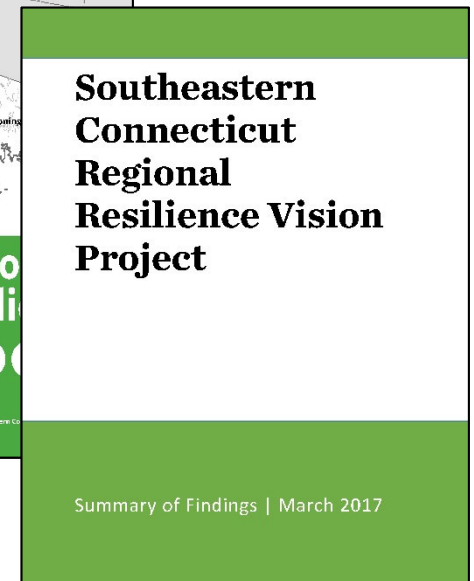


Groton Plan of Conservation & Development 2016



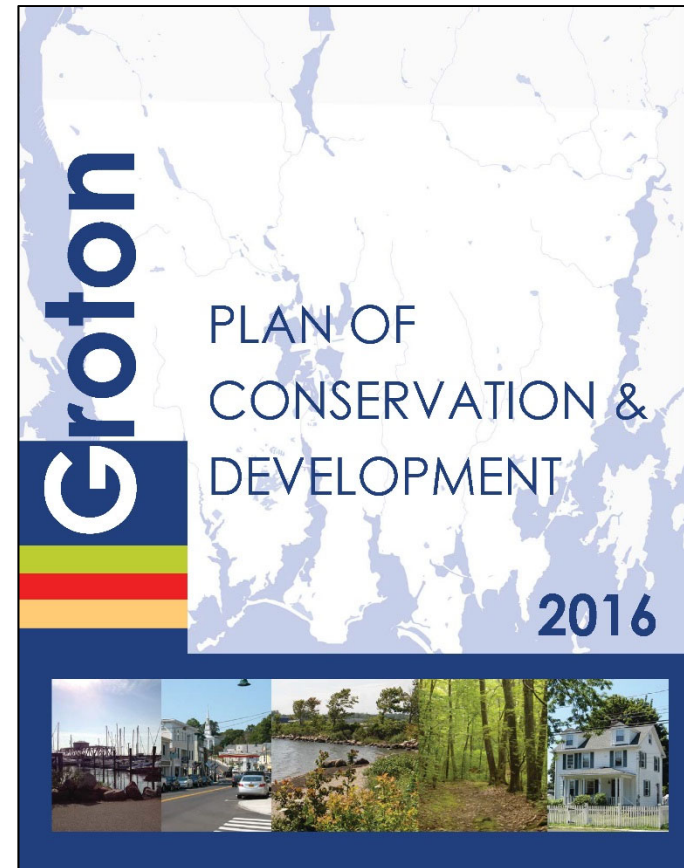
Southeastern Connecticut Regional Resilience Vision Project, Guidebook March 2017

Southeastern Connecticut Regional Resilience Vision Project, Summary of Findings March 2017



Groton Plan of Conservation and Development (POCD):

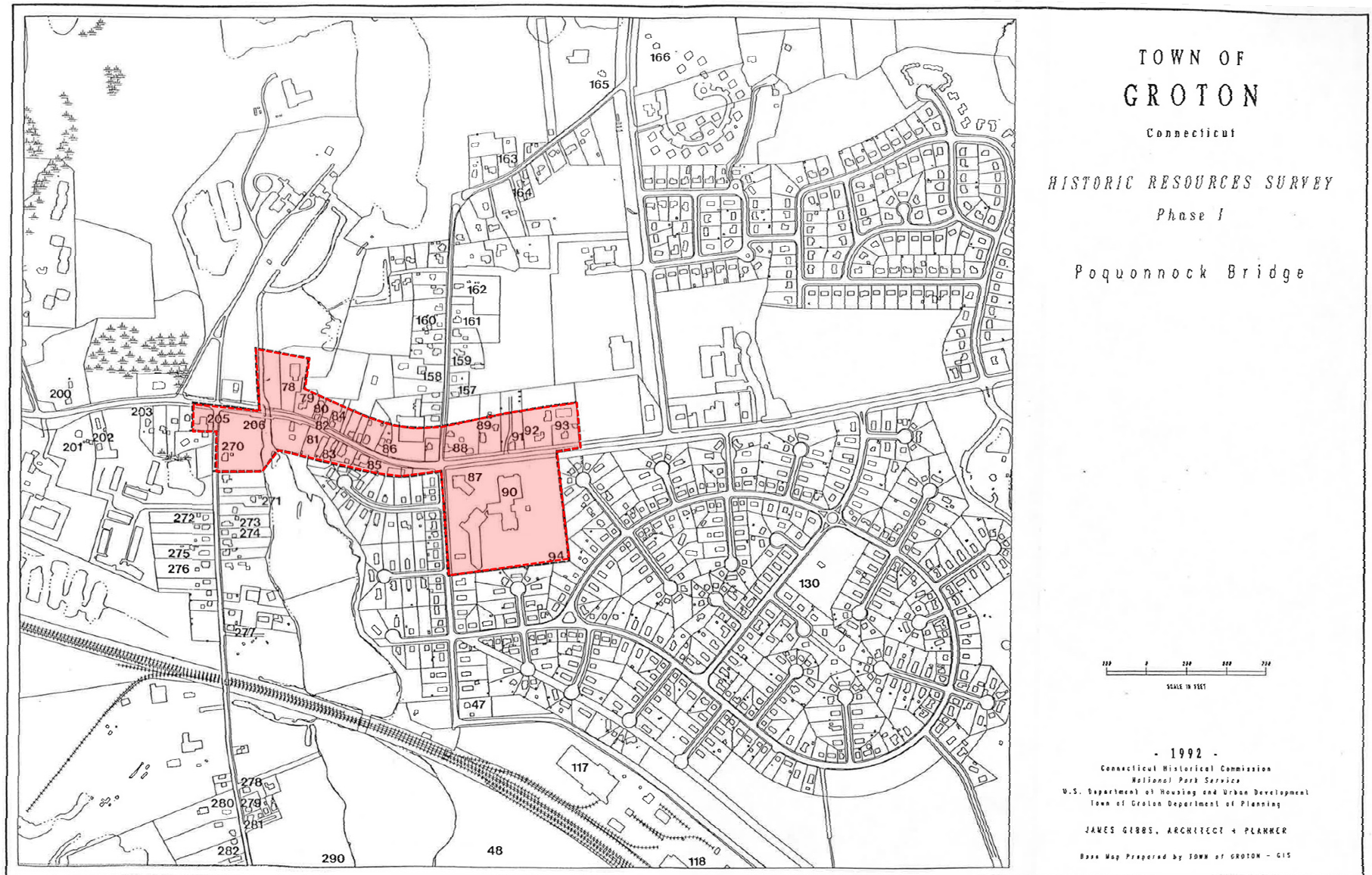
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- **Poquonnock Bridge identified as historic village center** where sense of place should be enhanced (protect historic, foster connectivity, mixed-use, pedestrian friendly, etc.) as it is the civic and governmental center of Groton



PROJECT BACKGROUND

POQUONNOCK BRIDGE VILLAGE

Historic Resources Survey:

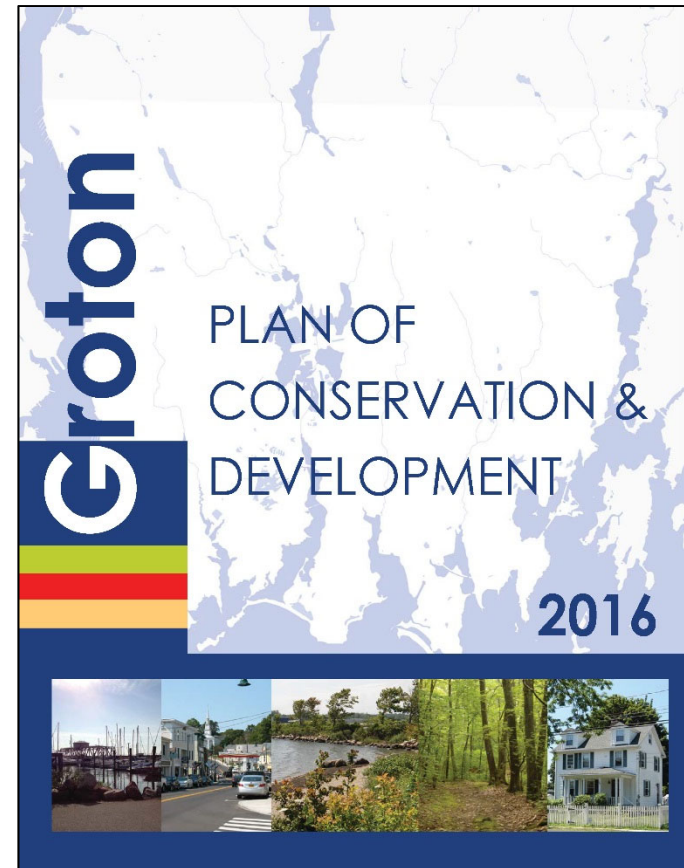


PROJECT BACKGROUND

POQUONNOCK BRIDGE VILLAGE

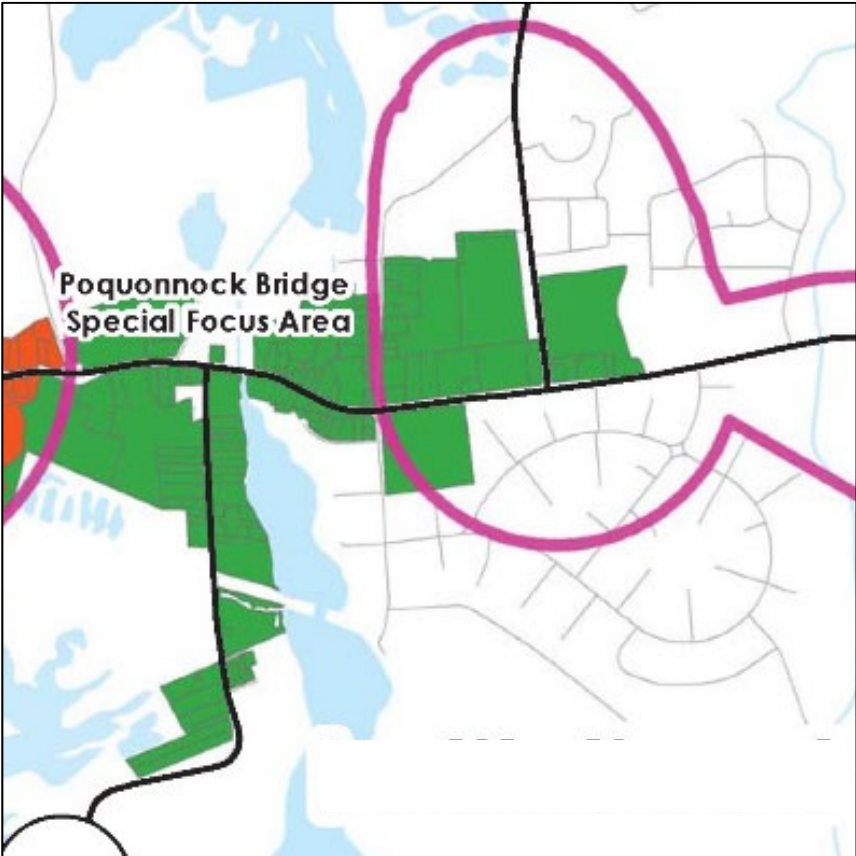
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- Poquonnock Bridge Village District identified as a **“Special Focus Area”**



Groton Plan of Conservation and Development (POCD):

Map D-7: Institutional Corridor/Poquonnock Bridge







Poquonnock Bridge Village District Special Focus Area

Purpose	Permit and control development within the designated Special Focus Area, which will protect and enhance historic village development patterns while building on the recommendations in the 1996 Historic Preservation Survey and protect the resources of the Poquonnock River.
Proposed Development Type	Village development pattern with historical architectural styles and massing, mixed uses, in a pedestrian-friendly environment.

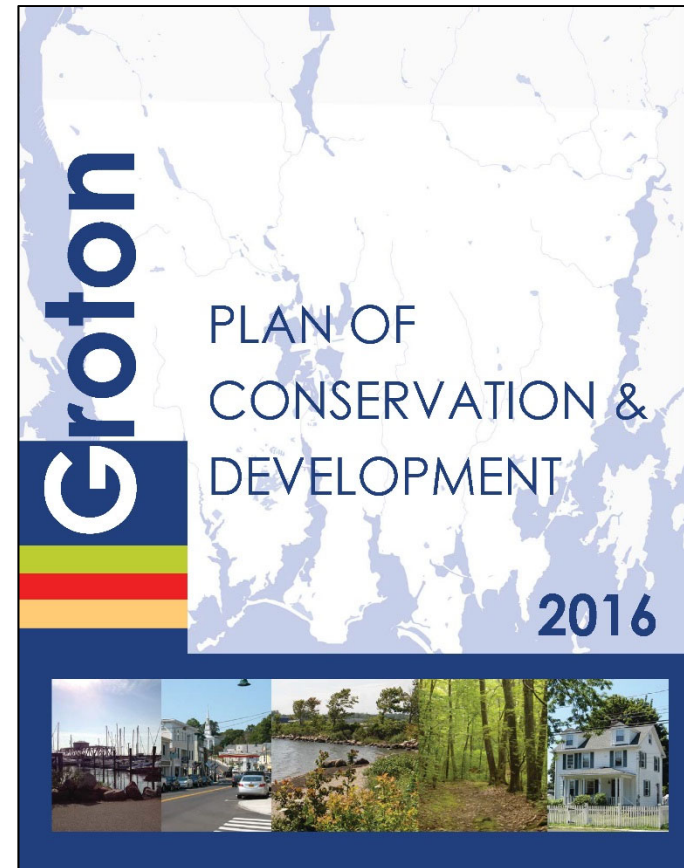
3-12

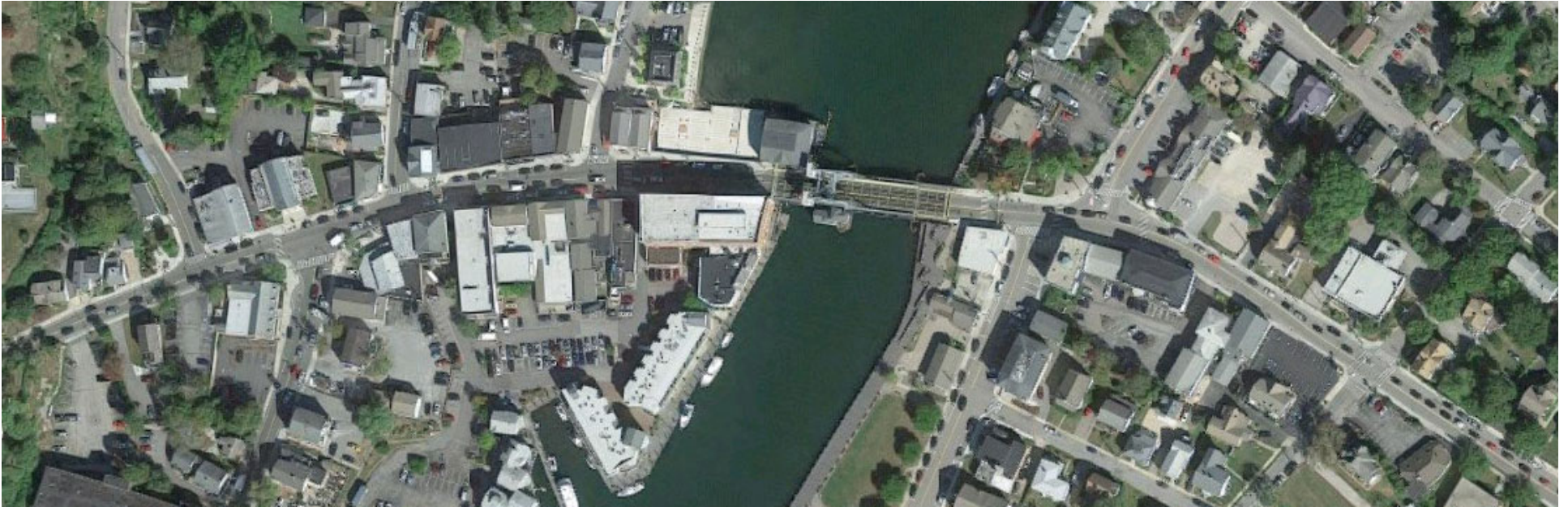
Groton
PLAN OF CONSERVATION + DEVELOPMENT
**Institutional Node and
Poquonnock Bridge
Special Focus Area**

-  Nodes
-  Downtown Special Focus Area
-  Poquonnock Bridge Special Focus Area
-  Other Jurisdictions

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PROJECT BACKGROUND

POQUONNOCK BRIDGE VILLAGE



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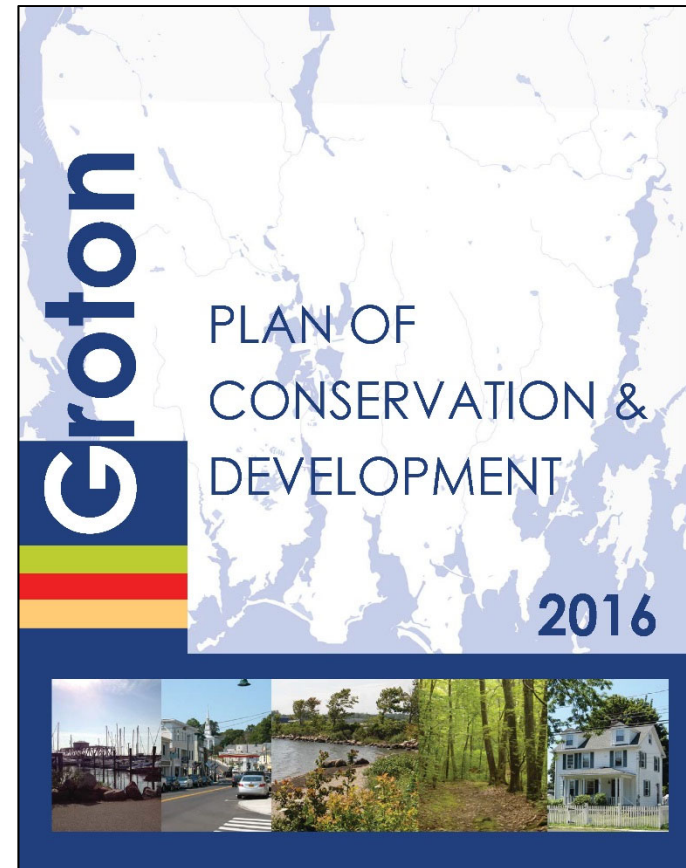


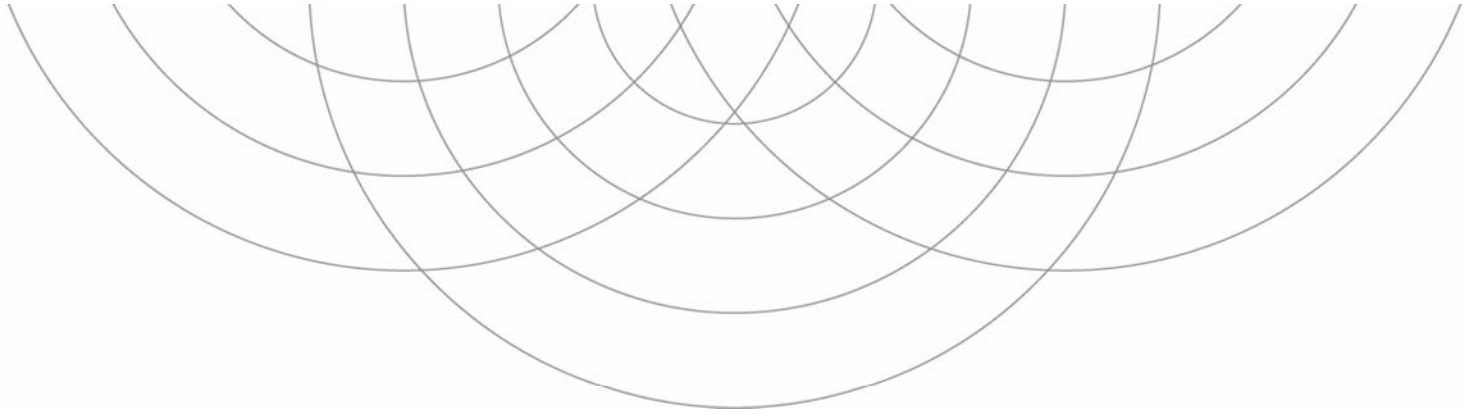
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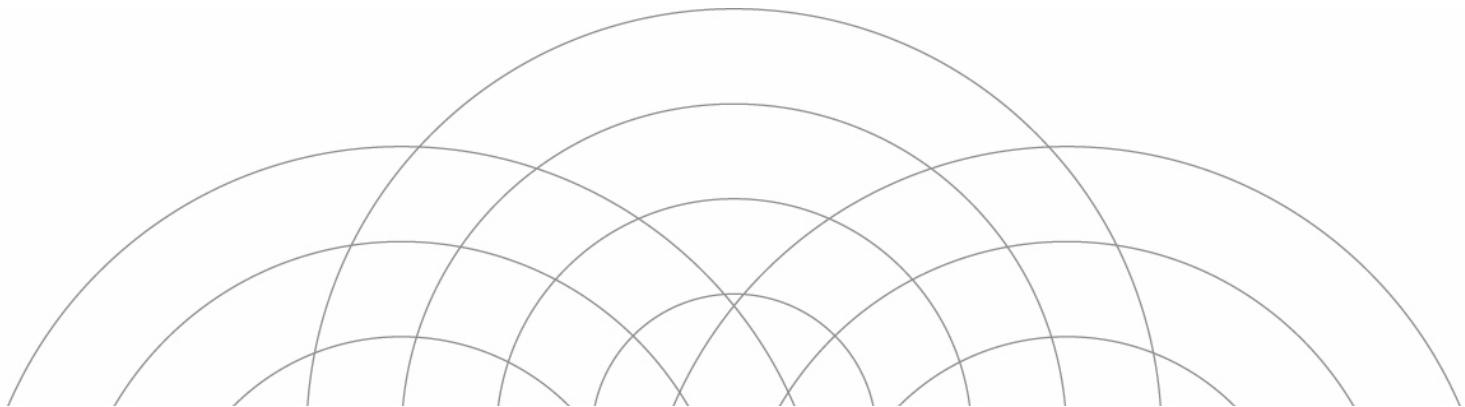
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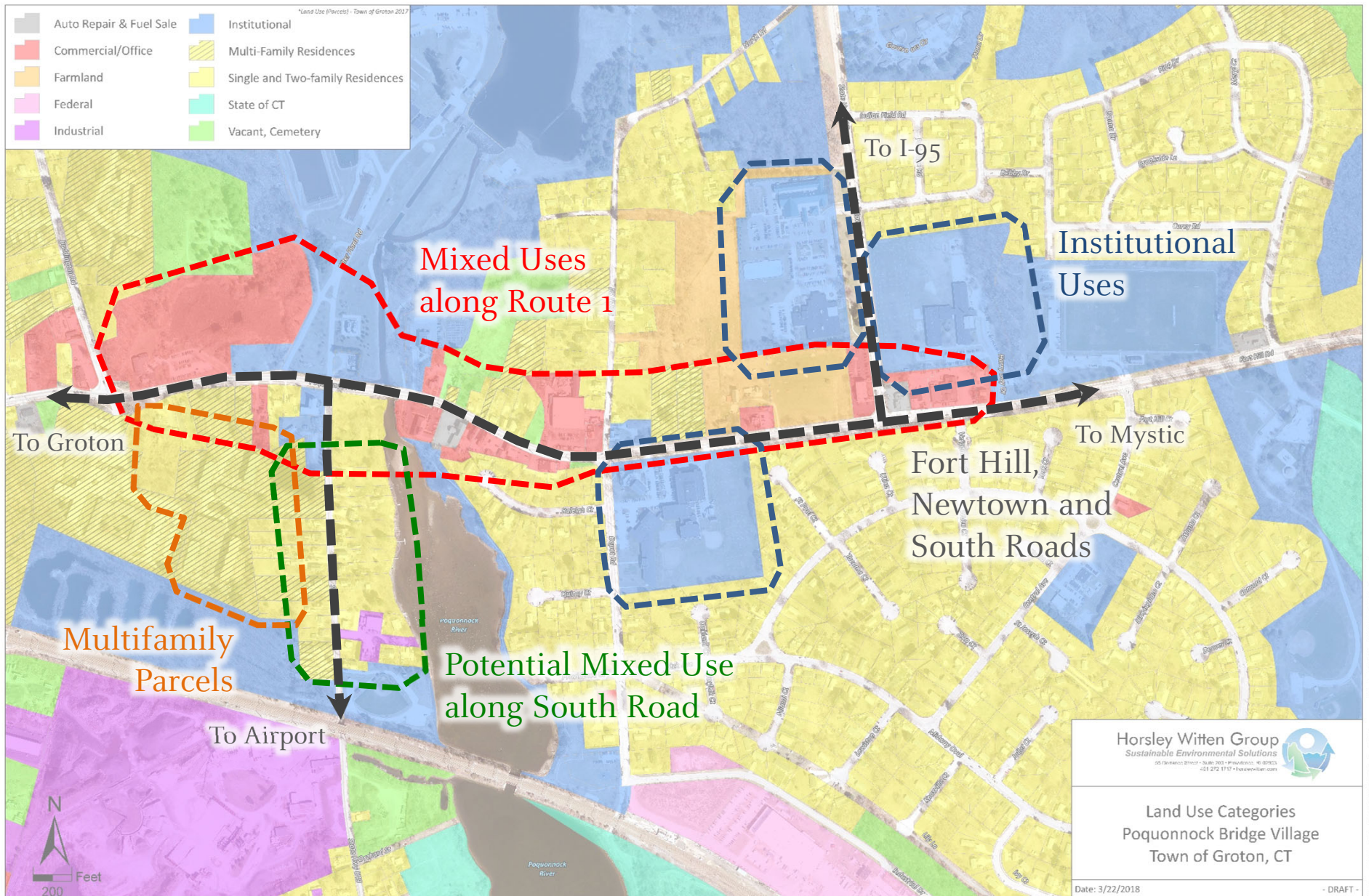
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- District should develop in a village pattern with historical styles and massing, mixed-use, pedestrian-friendly
- **Design guidelines are great tool for promoting appropriate development**





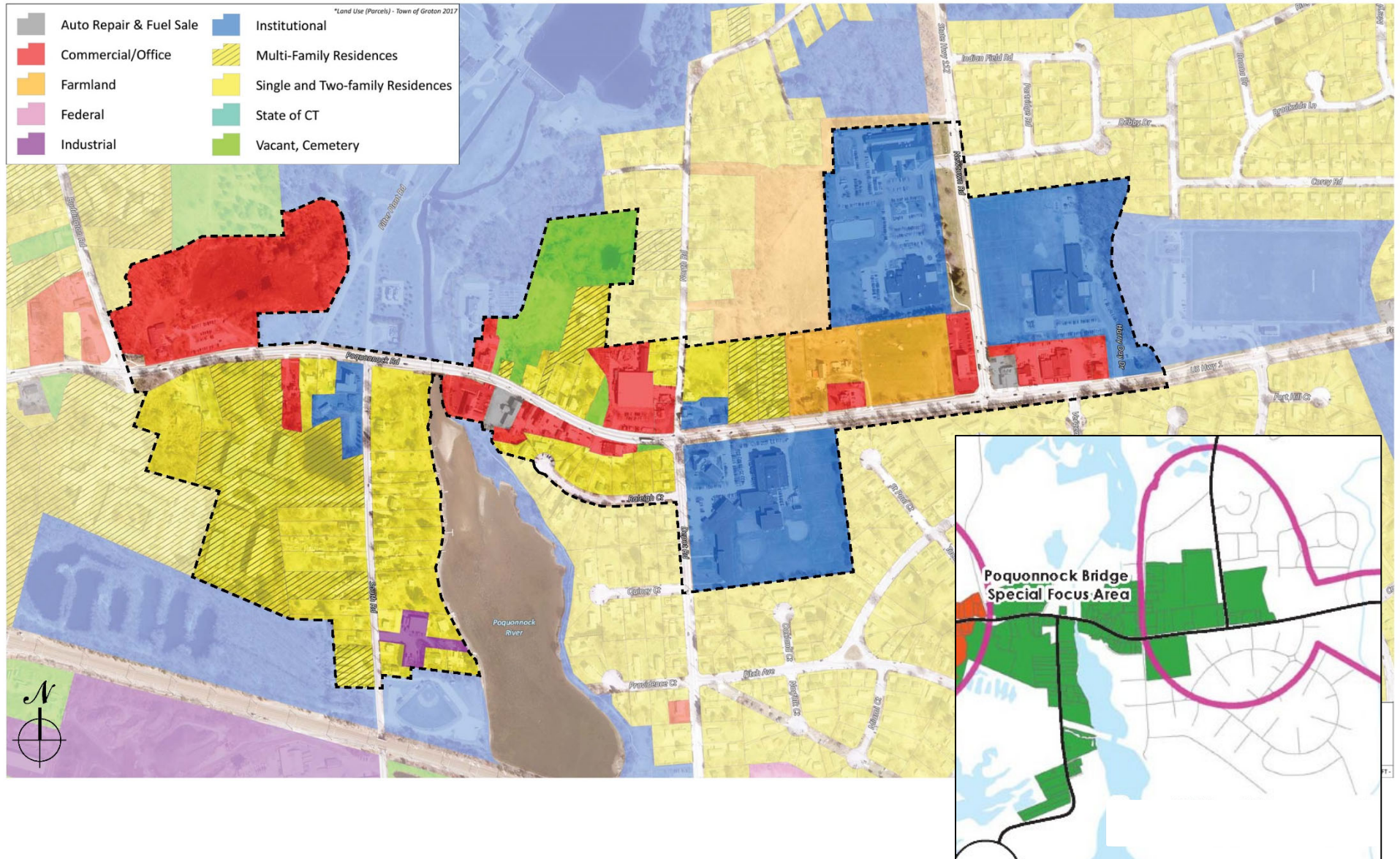
Recap of First Public Presentation:
Draft Design District





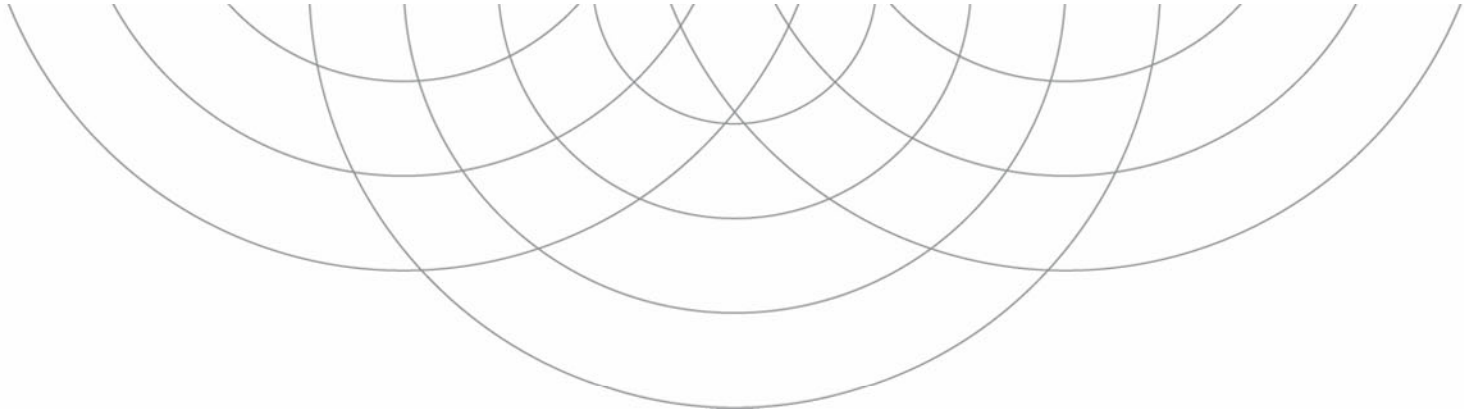
DESIGN DISTRICT - DRAFT

POQUONNOCK BRIDGE VILLAGE

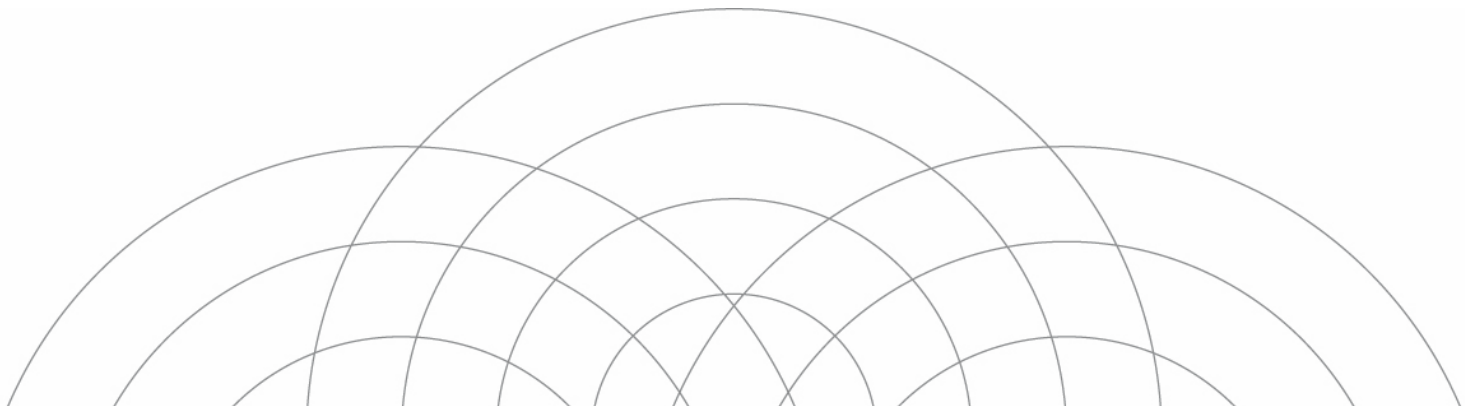


DESIGN DISTRICT - DRAFT

POQUONNOCK BRIDGE VILLAGE

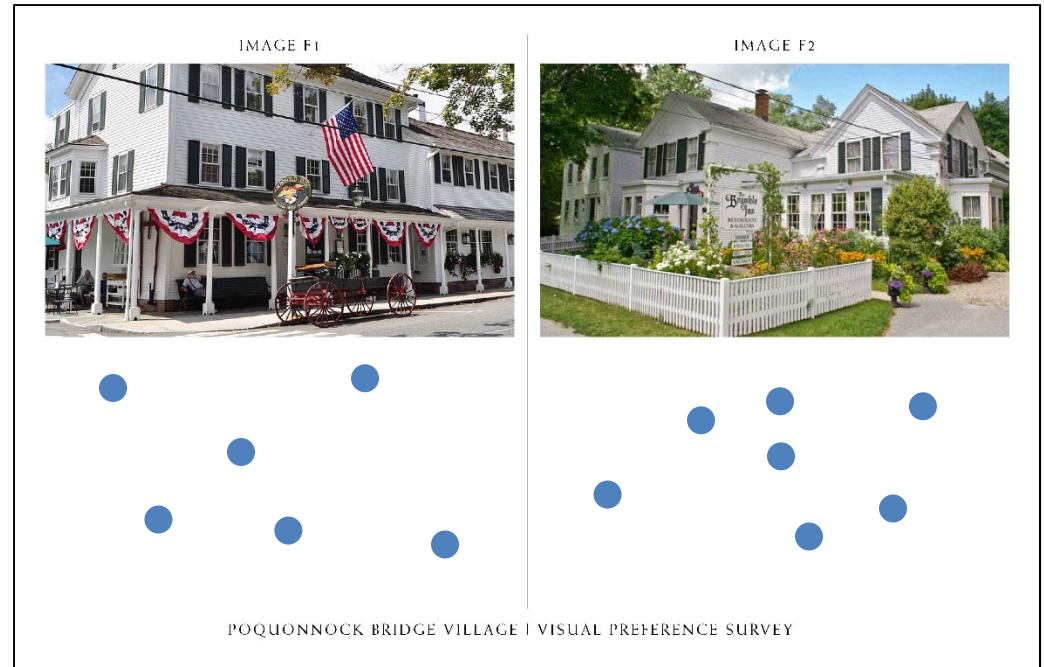


Visual Preference Survey



Visual Preference Survey:

- To help inform design guidelines, as last step would like your help with a visual preference survey.
- Each board contains two images – **place a single dot** on the image you find preferable. There are no wrong answers.
- If you'd like, please **leave a note on an index card** sharing your thoughts.
- The team will also be around if you want to **ask questions or discuss**.



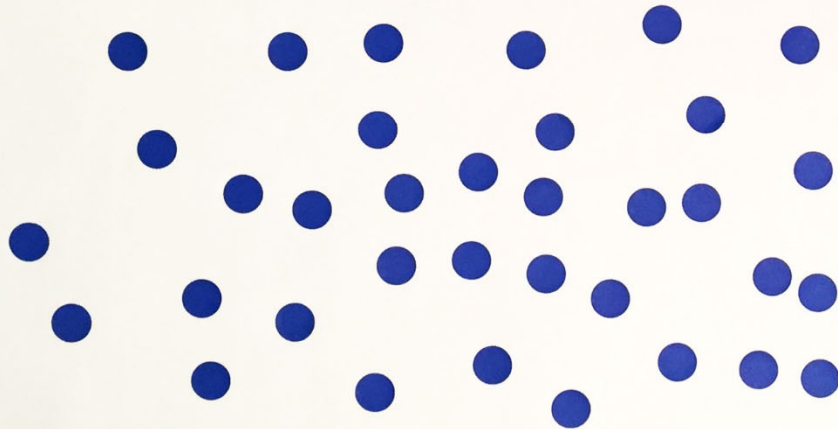
VISUAL PREFERENCE SURVEY

POQUONNOCK BRIDGE VILLAGE

IMAGE A1



IMAGE A2



POQUONNOCK BRIDGE VILLAGE | VISUAL PREFERENCE SURVEY

35 – 1 – 2
(38 Total Votes)

VISUAL PREFERENCE SURVEY

POQUONNOCK BRIDGE VILLAGE

IMAGE B1



IMAGE B2



POQUONNOCK BRIDGE VILLAGE | VISUAL PREFERENCE SURVEY

16 – 3 – 18
(37 Total Votes)

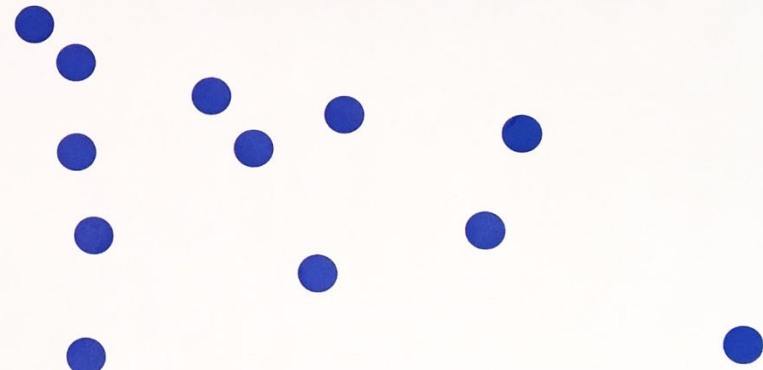
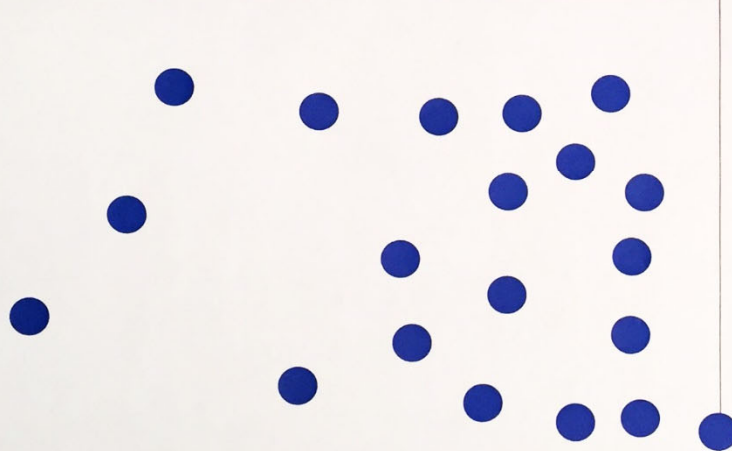
VISUAL PREFERENCE SURVEY

POQUONNOCK BRIDGE VILLAGE

IMAGE C1



IMAGE C2



POQUONNOCK BRIDGE VILLAGE | VISUAL PREFERENCE SURVEY

19 – 1 – 12
(32 Total Votes)

VISUAL PREFERENCE SURVEY

POQUONNOCK BRIDGE VILLAGE

IMAGE D1



IMAGE D2



POQUONNOCK BRIDGE VILLAGE | VISUAL PREFERENCE SURVEY

10 – 10 – 19
(39 Total Votes)

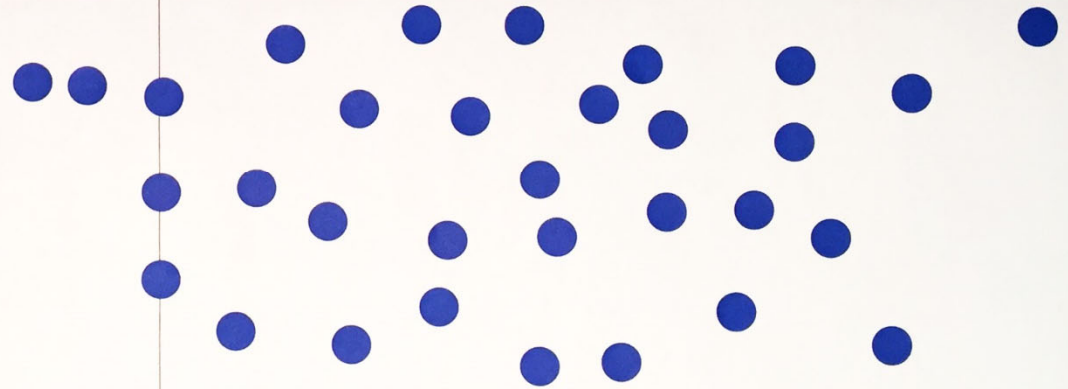
VISUAL PREFERENCE SURVEY

POQUONNOCK BRIDGE VILLAGE

IMAGE E1



IMAGE E2



POQUONNOCK BRIDGE VILLAGE | VISUAL PREFERENCE SURVEY

2 - 3 - 27
(32 Total Votes)

VISUAL PREFERENCE SURVEY

POQUONNOCK BRIDGE VILLAGE

IMAGE F1



IMAGE F2



POQUONNOCK BRIDGE VILLAGE | VISUAL PREFERENCE SURVEY

12 - 3 - 23
(38 Total Votes)

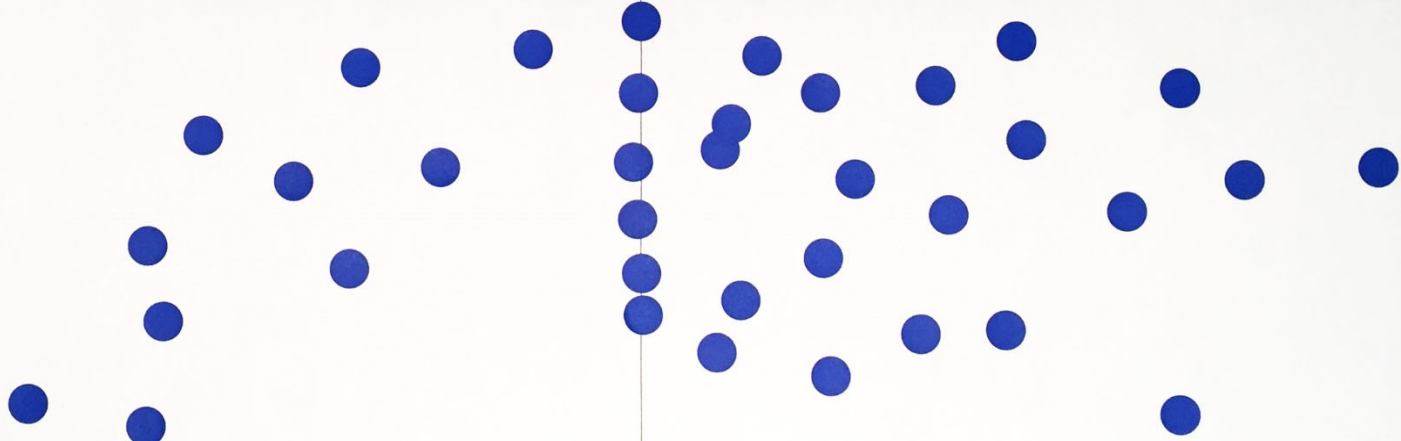
VISUAL PREFERENCE SURVEY

POQUONNOCK BRIDGE VILLAGE

IMAGE G1



IMAGE G2



POQUONNOCK BRIDGE VILLAGE | VISUAL PREFERENCE SURVEY

10 – 6 – 20

(36 Total Votes)

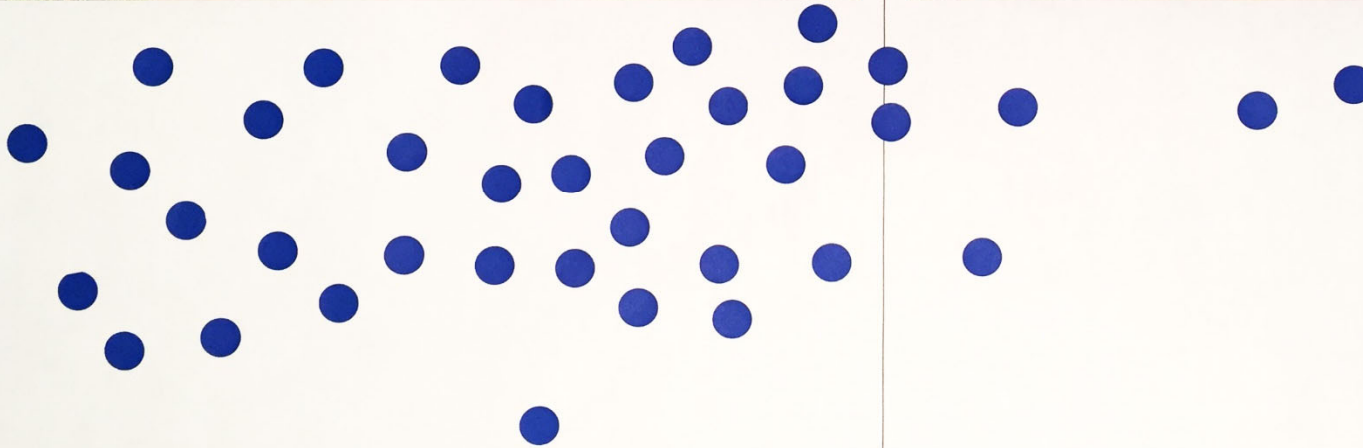
VISUAL PREFERENCE SURVEY

POQUONNOCK BRIDGE VILLAGE

IMAGE H1



IMAGE H2



POQUONNOCK BRIDGE VILLAGE | VISUAL PREFERENCE SURVEY

32 - 2 - 4
(38 Total Votes)

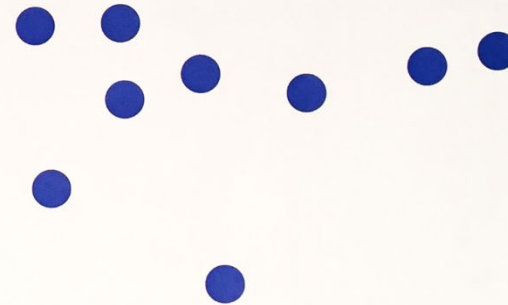
VISUAL PREFERENCE SURVEY

POQUONNOCK BRIDGE VILLAGE

IMAGE 11



IMAGE 12



POQUONNOCK BRIDGE VILLAGE | VISUAL PREFERENCE SURVEY

27 - 0 - 9
(36 Total Votes)

VISUAL PREFERENCE SURVEY

POQUONNOCK BRIDGE VILLAGE

IMAGE J1



IMAGE J2



POQUONNOCK BRIDGE VILLAGE | VISUAL PREFERENCE SURVEY

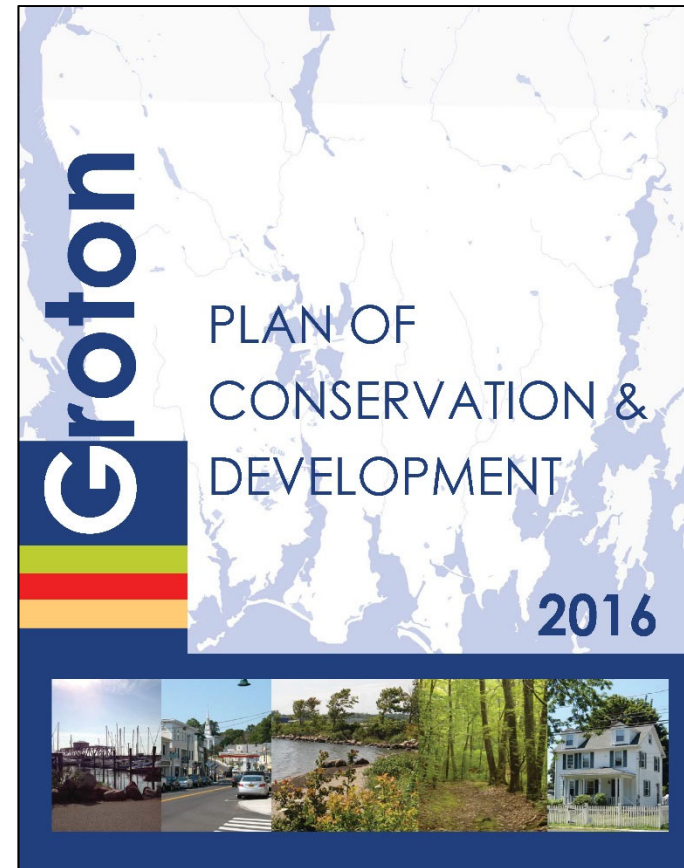
3 - 2 - 23
(28 Total Votes)

VISUAL PREFERENCE SURVEY

POQUONNOCK BRIDGE VILLAGE

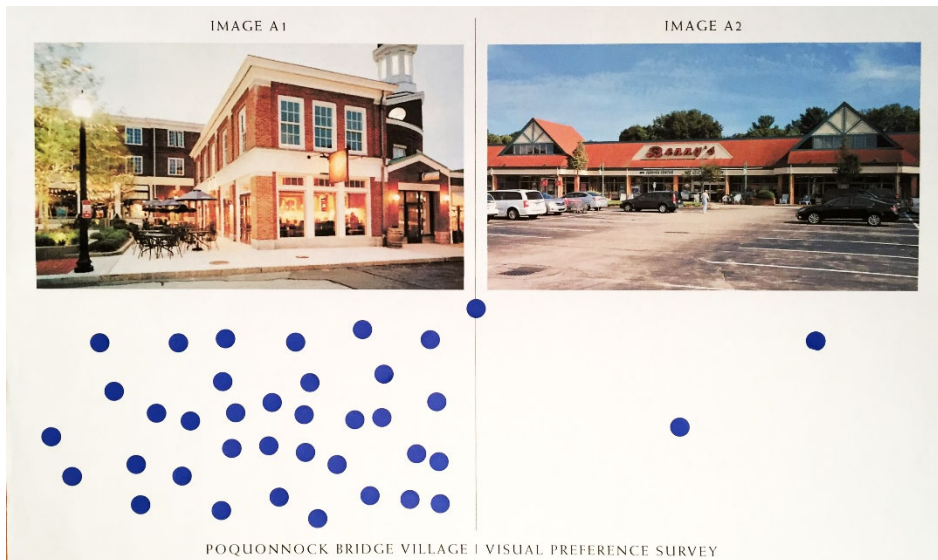
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Village Pattern vs. Sprawl

As evidenced by A1/A2 comparison and to a lesser degree C1/C2, there seems to be a preference for the village pattern of development – walkable streets with incremental development. This is the direction promoted by the POCD and the community was generally supportive of this in comments and discussion.



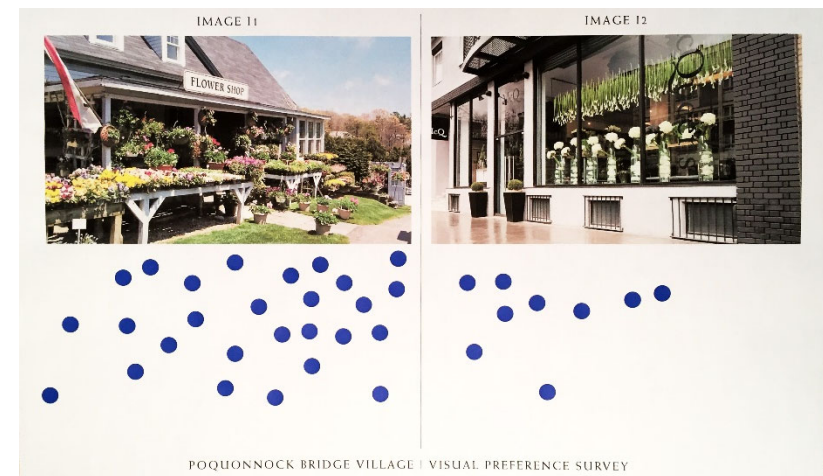
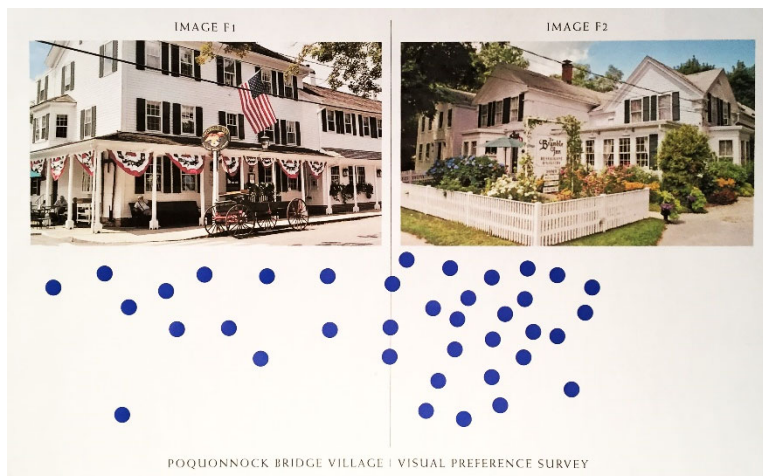
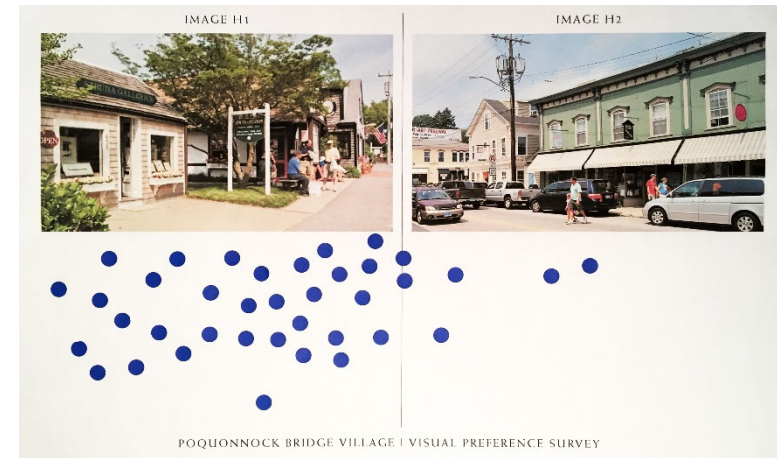
VISUAL PREFERENCE SURVEY



POQUONNOCK BRIDGE VILLAGE

Main Street vs. Rural Village

As evidenced by D1/D2, F1/F2, H1/H2, and I1/I2, there is a clear preference for the more informal character of a rural village than the more formal character of a traditional main street. There were also several comments relative to not wanting to be another Mystic. Poquonnock Bridge Village historically was a rural village, so this more informal form is more appropriate to continue that tradition.

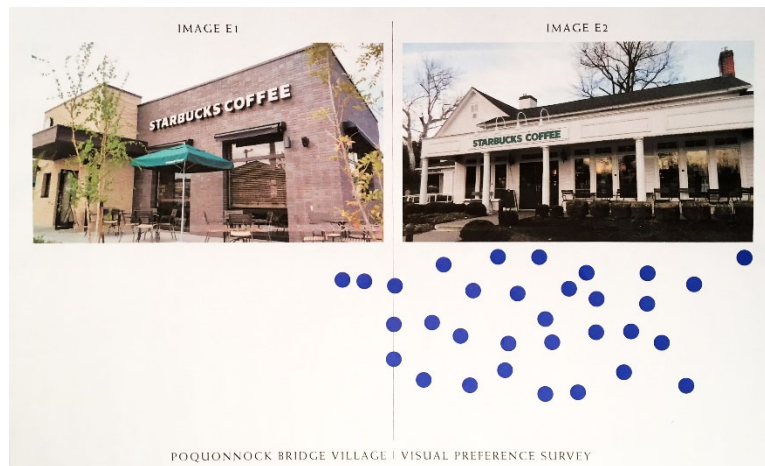
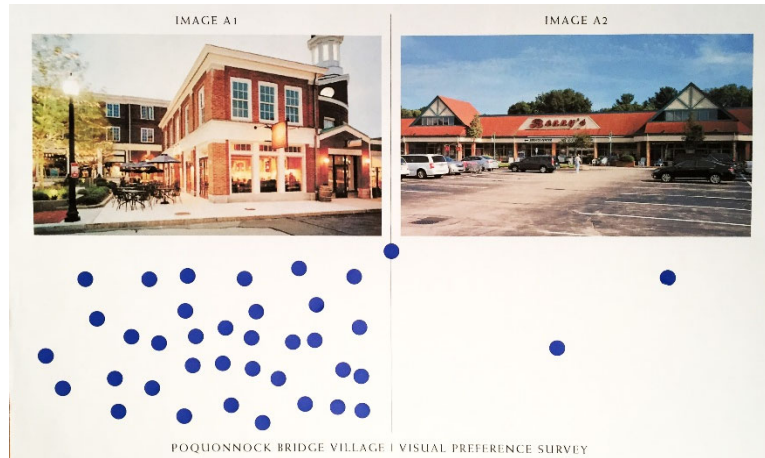


VISUAL PREFERENCE SURVEY

POQUONNOCK BRIDGE VILLAGE

Traditional vs. Contemporary Architecture

As evidenced by A1/A2, E1/E2, I1/I2, and to a lesser degree C1/C2, there is a clear preference for more traditional styles and forms of architecture. Given the existing building fabric of Poquonnock Bridge, this also seems like the appropriate direction to assure future development fits in with the local context.



VISUAL PREFERENCE SURVEY

POQUONNOCK BRIDGE VILLAGE

Scale and Character of Architecture

In general images that include 1.5-2.5 story, mixed-use precedents were preferred to single story, commercial uses. This can be seen in A1/A2, and C1/C2. The split vote for B1/B2 suggests no strong preference in the 1.5 – 3 story range, or in terms of how varied or cohesive the materials and colors of the individual buildings are.



VISUAL PREFERENCE SURVEY

POQUONNOCK BRIDGE VILLAGE

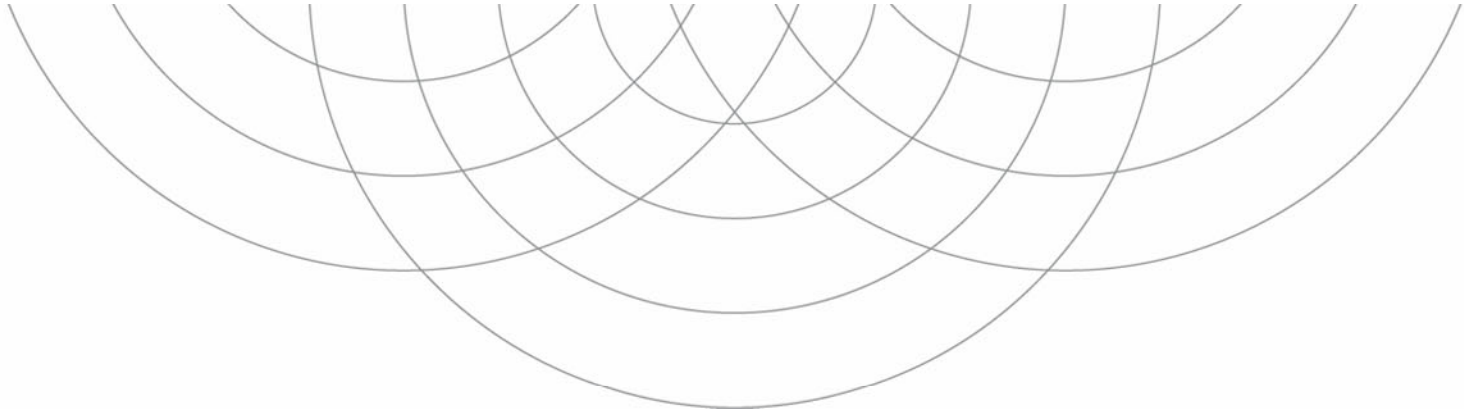
Impact of Automobiles

As evidenced by A1/A2, several comments received and possibly the results of C1/C2 (where the bias for traditional style might have suggested a stronger vote otherwise) and H1/H2 (where the strong bias towards one image may have been driven by the lack of cars), it is clear that the community has a preference for minimizing visual impacts and pedestrian safety concerns of vehicles within the village. That said, several comments also showed an understanding that vehicular circulation and adequate parking are “necessary evils” that just need to be tamed and/or located so as not to detract from the goal of a pedestrian friendly village.

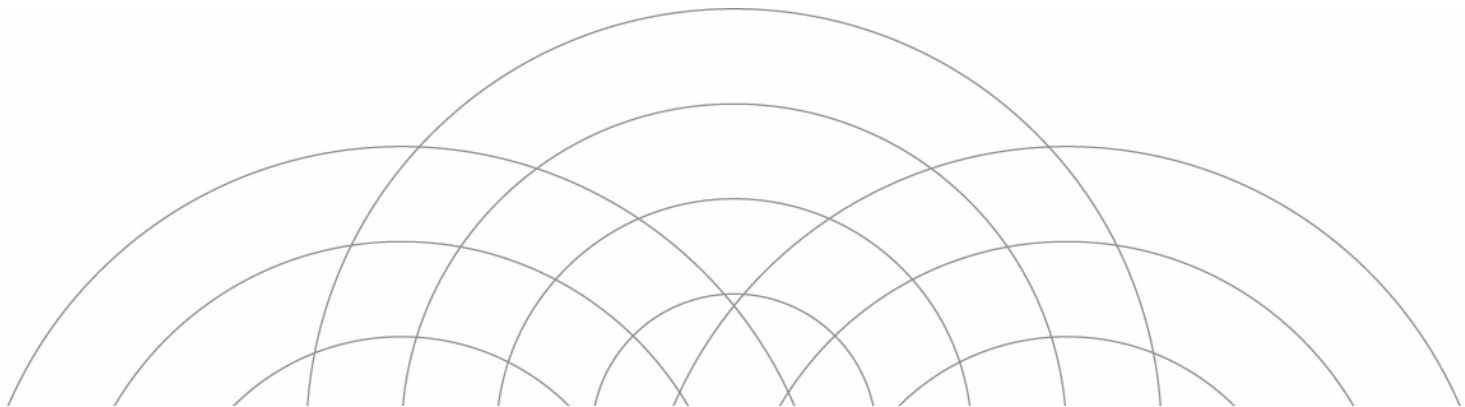


VISUAL PREFERENCE SURVEY

POQUONNOCK BRIDGE VILLAGE



Zoning Regulations Update



Poquonnock Village Zoning: The Must Haves

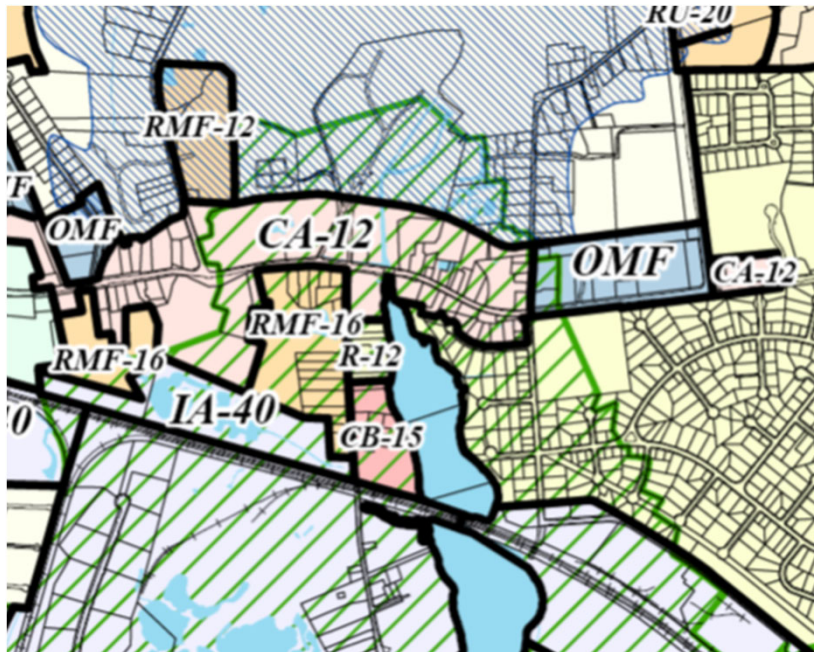
Zoning sets the *standards* for development that *must* be met



Proposed zoning for Poquonnock Village:
Mixed-Use Village Center (MVC)

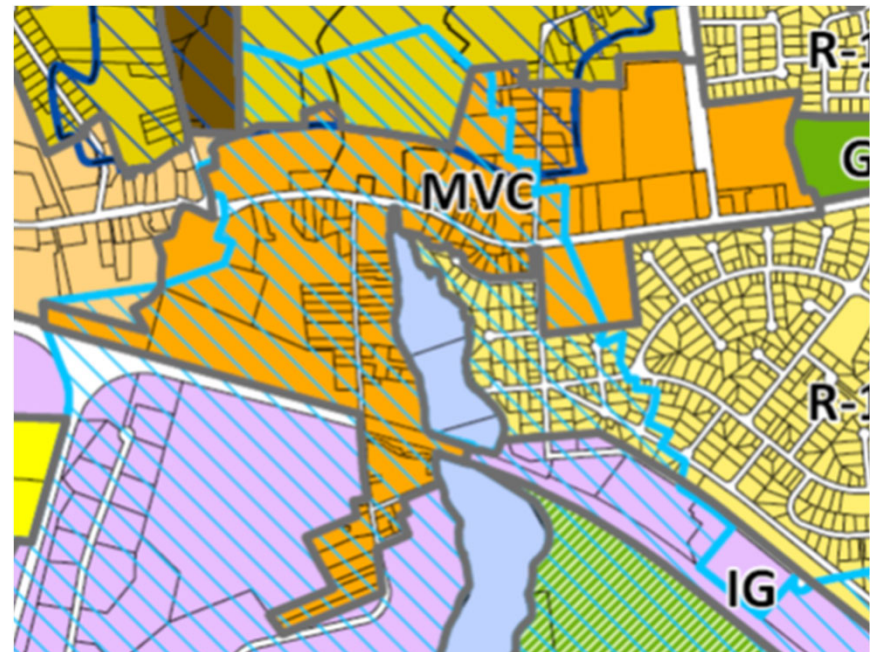
From a confusing mix of residential, commercial and industrial zones to a single, cohesive district

Poquonnock Area Zoning Today



Different standards from zone to zone

Proposed Poquonnock Area Zoning



Single set of standards for a mix of uses



MVC Zoning District: What it Manages

- Dimensional Standards (Lot size, setbacks, height, public space, etc.)
- Circulation
- Property Frontage
- Landscaping
- Parking & Loading
- Fencing & Screening
- Lighting
- Utility Lines
- Building Form
- Building Entranceways

Dimensional Standards

Minimum AND maximum front yard setbacks help make sure buildings are safe from the road, but not TOO far back



Lot Size

Minimum Lot Area: 5,000 SF

Minimum Lot Width: 80 feet

Setbacks

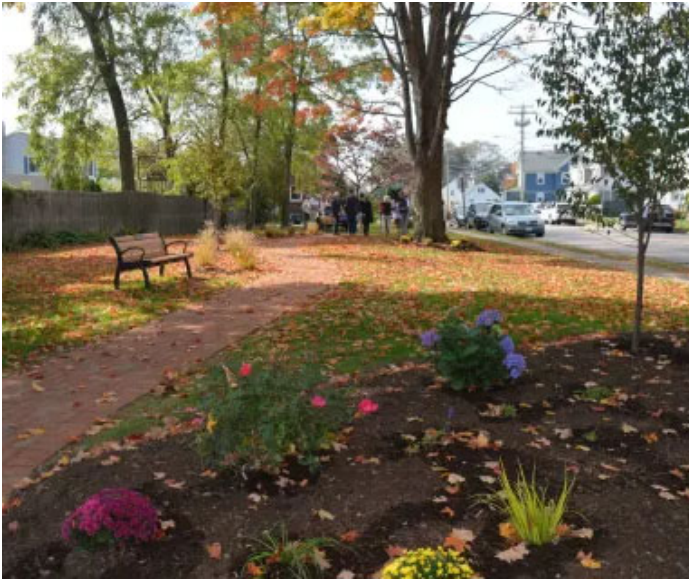
Different standards for . . .

- Properties on arterial roads (Route 1)
- Properties abutting residential districts or non-MVC districts
- Properties internal to the MVC district

Buildings can be attached at their sides with a Common Lot Line Agreement

Dimensional Standards

(continued)



Maximum Building . . .

Height: 35 feet

Coverage: 80%

Minimum Lot Area per Dwelling

None – if parking, height, etc. are met, the number of dwelling units are not artificially capped

Public Space

For sites of 2+ acres only, 10% set aside for publicly accessible space

Applicability

When do these standards apply?



Any new development where a site plan is reviewed by the Planning & Zoning Commission
(does not apply to single family homes)

Minor additions (over threshold TBD)

If you aren't making any such changes, these standards do NOT apply to you.

Circulation

Pedestrian and Bikeways must be clearly marked between sidewalks and buildings.



Frontage

Areas in front of buildings must be designed for pedestrian and bicycle access. *Along Route 1, 10-foot wide sidewalks required*

Parking & Loading

Located behind or to the side. Use 5-foot wide landscape area or 3-foot high decorative wall between sidewalks and existing parking areas.



Fencing & Screening

Chain link is prohibited in front and side yards, unless necessary for security and screened with evergreens.

Lighting & Utilities

Light poles no higher than 20 feet.

Utility lines and cables laid underground



Building Entranceways

Main entry must face a street or a 10-foot wide sidewalk. Entrance must be architecturally obvious. Buildings on Route 1 must have a primary entrance facing Route 1.

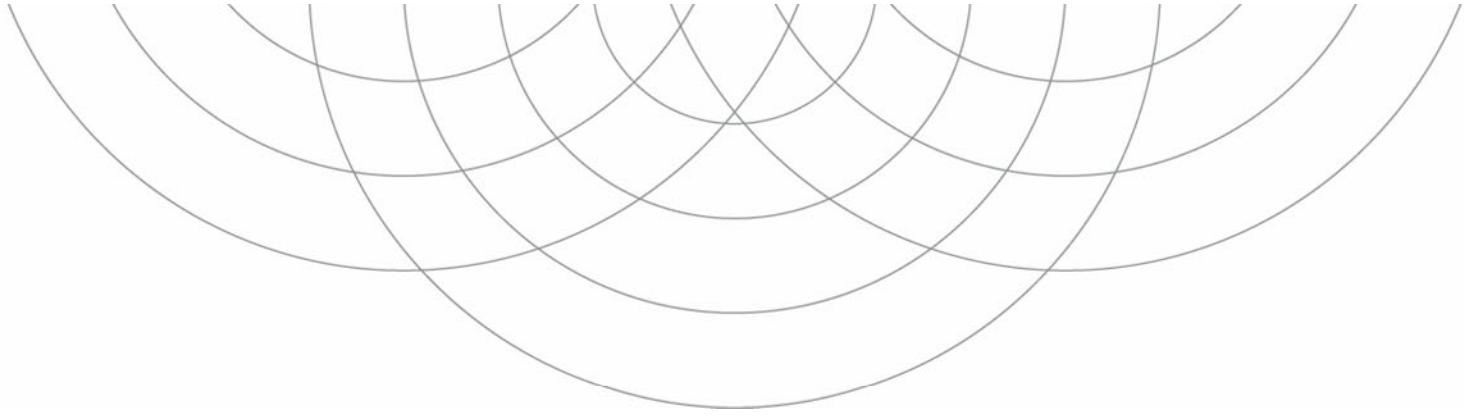
Building Form

For new buildings:

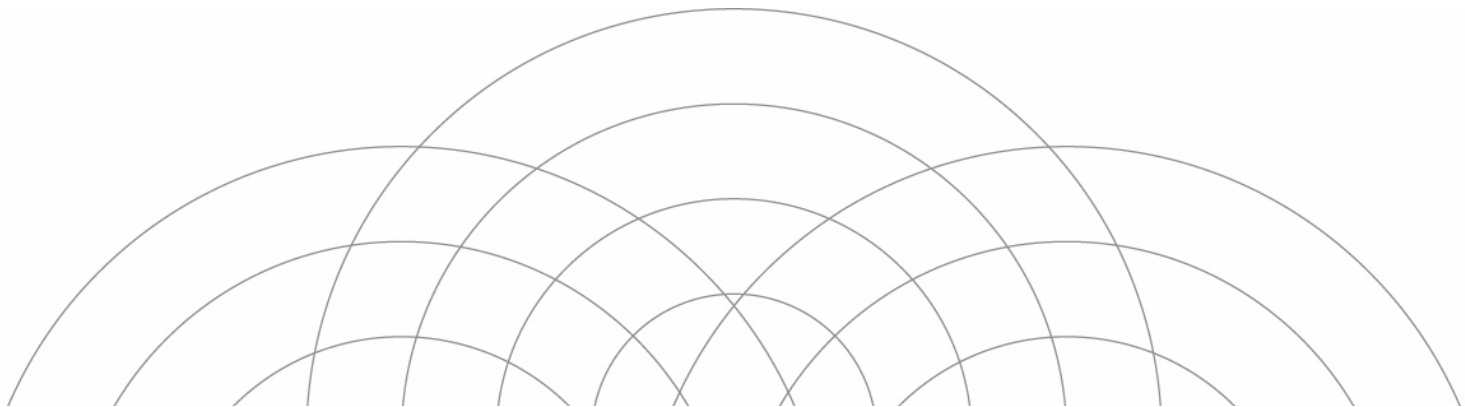
Base, middle, and top of the building must be obvious. First floor must be 11 feet tall, and the front of the building must *look* like the front.



Buildings with façades longer than 40 feet must add architectural features so that you don't have long, blank walls.



Draft Design Guidelines



Poquonnock Village Zoning: The Must Haves

Zoning sets the *standards* for development that *must* be met



Proposed zoning for Poquonnock Village:
Mixed-Use Village Center (MVC)

Poquonnock Village Zoning: The Must Haves

Zoning sets the *standards* for development that *must* be met
Design Guidelines illustrate the *principles* that *should* be met



Proposed zoning for Poquonnock Village:
Mixed-Use Village Center (MVC)

Zoning Regulations and Design Guidelines work hand in hand, with the ultimate goal being to assure renovations to existing structures and/or new infill development **respect and build upon the existing historic and walkable character** of Poquonnock Bridge.

MVC Zoning District:

- **Dimensional Standards** (Lot size, setbacks, height, public space, etc.)
- **Circulation**
- **Property Frontage**
- **Landscaping**
- **Parking & Loading**
- **Fencing & Screening**
- **Lighting**
- **Utility Lines**
- **Building Form**
- **Building Entrancesways**

Design Guidelines:

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Design Guidelines Format

Importance of Considering Particular Design Element

Key Features and Examples Showing Proper Application

Design Guidelines Section Being Addressed

Approaches and Examples Showing Improper Application

Design Element Being Addressed

LIGHTING

STREET LIGHTING IS A KEY COMPONENT TO THE MAKING OF A PLACE

Street lighting has a strong effect on the character of a place. This includes both the distribution of the light, and the character of the light fixture/pole itself.

A mix of lighting types, including appropriate street lights, helps to establish the character of a place.

DO

Key Features

- A variety of lighting types should be used to address the different needs of civic spaces, pedestrian oriented streets, and parking areas.
- Lighting should be designed to provide a uniform distribution of light while also addressing safety needs.
- Lighting should be in keeping with the character of the spaces and buildings around them. Industrial poles and fixtures concerned purely with functional requirements should be avoided.

AVOID

Designing site lighting purely to meet functional requirements denies its ability to support character of place.

SITE COMPONENTS

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Downtown Groton Design Guidelines

Site Design

BUILDING LOCATION

ON A TRADITIONAL MIXED-USE STREET, BUILDINGS ENGAGE THE STREET

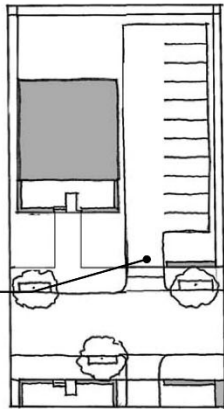
Buildings may have varying setbacks and an informal arrangement, but should engage the street and contribute to the character of the public realm



Buildings in a village have varying setbacks but still relate to the street.

DO

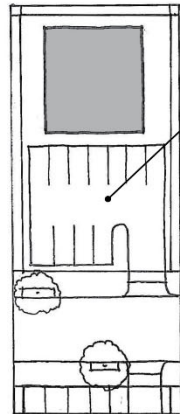
Buildings should repeat the pattern of the surrounding district. A reasonable setback from the road behind a layer of landscape or casual seating is appropriate



Landscaping or seating in the front setback provide visual interest for passing pedestrians and potential customers.

Page 10

AVOID



Front setback area typically used for parking is hostile to pedestrians.



Retail buildings set back from the street discourage shoppers since even adjacent shops or those across the street are separated by expanses of cars and asphalt.

Poquonnock Bridge Village Design Guidelines

BUILDING ENTRY LOCATION

BUILDINGS SHOULD BE ENTERED DIRECTLY FROM THE SIDEWALK

Building entries should engage and activate the street edge. In mixed-use conditions, there should also be a clear separation and hierarchy between retail storefronts and secondary entries for upper floor uses.



Commercial entries connect directly to a public sidewalk



DO

Secondary entrance / exits may be located facing the rear parking area, but retail entrances should never solely be located at the rear.

Provide a clear walking path from the rear parking area to the front entrance.

A corner entrance can provide both main thoroughfare entry as well as access from rear parking.

Retail entrances should always be located fronting the main thoroughfare.

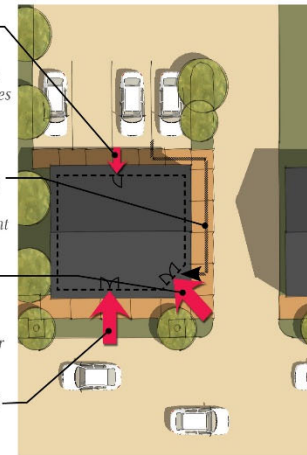


Diagram of entry location(s).

Key Points

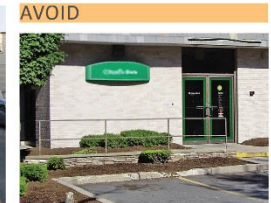
- Entries to retail and commercial uses should be prominent and large in scale to be clearly identifiable to the public.
- Retail entries should be located directly off the sidewalk. Galleries and indoor malls are prohibited.
- Residential entries should be separate from public entries. The location may be less prominent and the scale smaller than for retail.
- Residential entries should have their own address separate from the retail.



An inset entry allows more display area and a protected place to view merchandise.



A corner entry serves customers arriving from two different directions.



Avoid locating the primary entry at the rear of a building, regardless of its proximity to a rear parking area.

AVOID

Poquonnock Bridge Village Design Guidelines

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Site Design

PARKING LOCATION

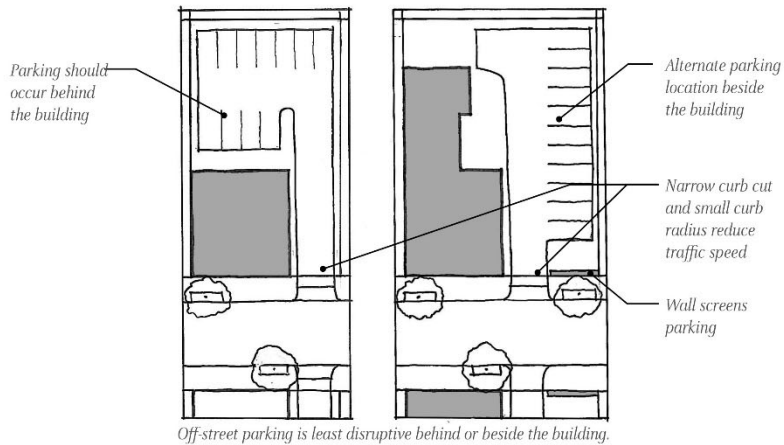
VISIBLE OFF-STREET PARKING HARMS TRADITIONAL MIXED-USE STREETS.

Off-street parking should be hidden to the greatest extent possible by buildings, fences, walls or landscaping. See Site Components section for more details on screening and landscape.



Successful mixed-use streets prioritize the connection between building frontages and the sidewalk, not front-loaded parking lots

DO



AVOID



Avoid parking lots in front of the building.



Wide entry and exit lanes, yield conditions and large curb radii allow traffic to enter or exit parking lots at dangerous speeds.

PARKING LOCATION

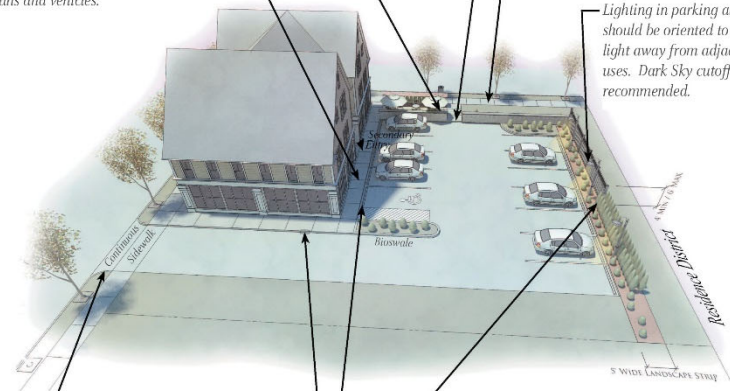
Screen parking areas from outdoor seating and sidewalks by the use of masonry walls and plantings at a minimum of three feet in height.

The layout and design of all means of circulation, including interior drives, parking areas, and walkways, should provide for safe, delineated circulation for pedestrians and vehicles.

Where parking abuts a sidewalk, a curb at least six inches high should be included to delineate the two uses.

Sidewalks or walkways should be included in commercial areas to provide a safe route from parking areas and retail establishments.

Lighting in parking areas should be oriented to reflect light away from adjacent uses. Dark Sky cutoff fixtures recommended.



Site entrances should be minimized, including sharing access drives with adjacent properties and creating contiguous parking areas wherever possible.

Opaque fencing and/or dense landscaping should be included to screen parking areas from adjacent residential areas.

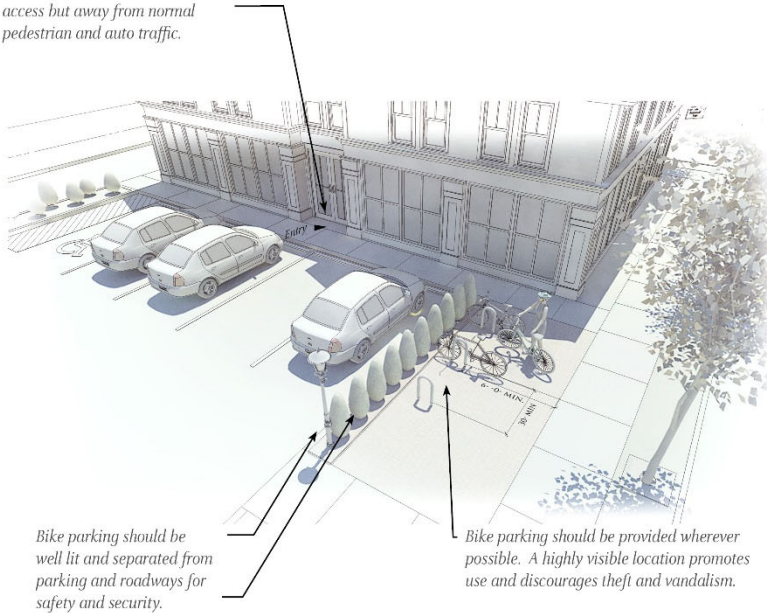
Sidewalks should be a hard surface with provisions for managing stormwater runoff included as necessary. Where sidewalks intersect with automobile lanes, raised surfaces and/or durable, decorative alternatives to conventional pavement shall be used to connect sidewalks or bike lanes across the automobile lanes.

Site Design

BIKE PARKING

SAFE AND CONVENIENT PARKING CAN ENCOURAGE BICYCLE USE.

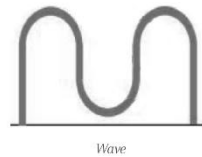
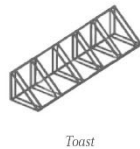
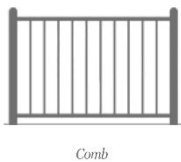
Bike parking should be convenient to building entrances and street access but away from normal pedestrian and auto traffic.



DO



AVOID



SIDEWALKS

WELL DESIGNED SIDEWALKS ARE CRUCIAL TO A PEDESTRIAN FRIENDLY ENVIRONMENT.

Sidewalks support pedestrian flow and mixed-use activity while also accommodating elements such as street trees, lighting and other street furniture.



Well designed sidewalks promote pedestrian use by a range of people, supporting local business, reducing vehicular use, and improving wellness.

DO



Key Features

- Sidewalks are composed of three zones: a furnishing zone (for trees, lighting, etc.), a pedestrian zone (for movement), and a frontage zone (for activities adjacent to building).
- Ideally each of these has a dedicated area.
- In highly constrained locations, these activities may overlap but doing so will constrain use and may eliminate the ability to include certain elements, such as street trees.

In addition to pedestrian movement, sidewalks can host a number of other amenities if given enough space and distributed appropriately.

AVOID



Narrow or discontinuous sidewalks end up compromised by other needs and don't promote pedestrian use.

Site Design

SIDEWALKS



Furnishing Zone
The area closest to the roadway, used for street signs, light fixtures, etc. This zone helps define the edge of the sidewalk area and protects pedestrians from vehicular traffic.

Pedestrian Zone
The middle of the sidewalk dedicated solely to pedestrian circulation. The pedestrian zone should be at least 5' for comfort and accessibility and should be free of obstructions.

Frontage Zone
The area of the sidewalk closest to the building face, used for objects and activities associated with the building. The frontage zone may include landscaping, outdoor seating, and temporary signage.



Furnishing zone used to define the edge of the sidewalk



Frontage zone used for plants and outdoor seating.

CIRCULATION



Continuous sidewalks promote pedestrian use by a range of people, supporting local business, reducing vehicular speed, and improving wellness.

DO



Raised and painted crosswalks serve as traffic calming measures by extending the sidewalk across the road and bringing vehicles to the pedestrian level.

SAFE, BROAD, AND CLEARLY IDENTIFIABLE PEDESTRIAN AND NON-MOTORIZED VEHICLE CONNECTIONS ENCOURAGE ALTERNATIVE MODES OF TRAVEL

The networks of circulation in mixed use areas should be designed to balance the needs of motorists, cyclists, and pedestrians.

Key Features

- Where sidewalks or other pedestrian or bicycle ways intersect with automobile driveways or lanes, raised surfaces and/or durable, decorative alternatives to conventional pavement should be used to connect sidewalks or bike lanes across the automobile lane.
- Striping across the asphalt used for an automobile lane to connect the pedestrian or bicycle way is not adequate.

AVOID



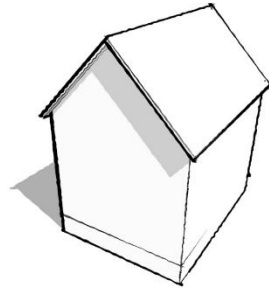
Crosswalks that are designed or located to prioritize automobile traffic create hazardous conditions for pedestrians.

Building Design

BUILDING FORM

TRADITIONAL BUILDINGS ARE COMPOSED OF SIMPLE VOLUMES

Simple massing was traditionally necessitated by the limited time, skill, and resources available. Simple forms translated into buildings that were economical for homeowners to build and easy to maintain. Complexity and interest occurred with the grouping of structures at the scale of the street or neighborhood rather than within a single building.



Simple rectangular volumes are functional and economical

DO

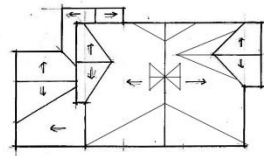
Key Points

- Traditional buildings take advantage of the efficiency and economy of simple building forms
- Roofs can help express the hierarchy of building volumes. Generally, a single dominant roof form is clearly legible, with the roofs of secondary volumes deferring in scale to the main body of the building.
- Good proportion and proper detailing can make even a basic form elegant



Classic main streets are comprised of several buildings, each with its own character but following general principles that create a cohesive whole.

AVOID



Avoid multiple changes in slope that are expensive to build and create a visually frenetic composition

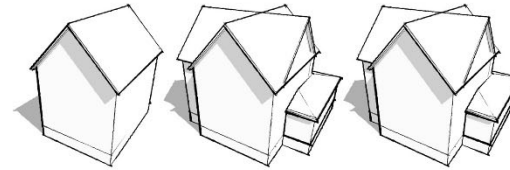


This building has been overly complicated by too many roof forms. The additional ridges, valleys and eaves are a maintenance liability.

BUILDING FORM

ADDITIONAL FORMS CAN ADD INTEREST AND SPACE TO A SIMPLE BUILDING

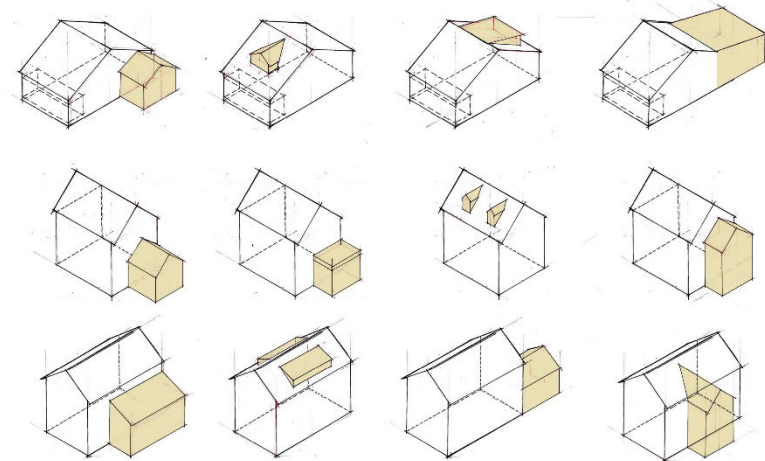
Traditional buildings change over time to accommodate the needs of new generations. Additions may provide for an expanding program, but always defer to the mass of the original building.



The transformation of a simply primary volume with a succession of secondary elements

DO

Historic buildings have a variety of additions, including new porches, wings, and dormers. The new spaces provide room for growth of the changing inhabitants and market trends.



Various ways to expand and transform an existing building. Additions are most successful when they defer in scale and proportion to the primary form of the original building. Secondary masses may also be used to compose a new building to create a modern structure with the character of a traditional home that has been around for generations.

Building Design

HISTORIC INFILL

WHEN SEEKING TO PRESERVE HISTORIC CHARACTER, NEW BUILDINGS SHOULD RESPECT EXISTING PATTERNS

New buildings in historic settings should attempt to reflect a similar scale and massing as the existing historic buildings.



New larger buildings can be designed to fit within a historic village setting.

DO

Key Points

- While building heights will vary by zone, new buildings in historic districts should respect the existing context
- Smaller buildings with useful spaces between are preferred over long continuous street facades



- Shed dormers provide opportunity for additional windows and increased head height on the top floor
- Top floor units concealed in roof form
- Keeping eave below third floor level reduces the scale of the building
- Ground floor activated and transparent to contribute to pedestrian friendly environment



A complex of smaller scale buildings is preferable to a single large structure because the varied massing provides visual interest and human scale

STOREFRONTS

GROUND-LEVEL RETAIL DRAWS SHOPPERS AND ENLIVEN THE SIDEWALK.

Large displays on a storefront can entice passers-by and invite them into a store. Visibility is important to make potential customers aware of a store's offering and create a sense of welcome.



DO



Transparency of a storefront invites customers by letting them know what is offered and if the shop is welcoming business.



Key Points

- Storefronts should contain approximately 70% clear glazed area for the display of goods and services.
- Displays should allow a view through to the sales floor for customers to easily see whether the shop is open.

AVOID



Opaque façades do not invite commercial activity.



The blank walls and windows are uninteresting and shoppers may pass on by.

Site Components

LIGHTING

STREET LIGHTING IS A KEY COMPONENT TO THE MAKING OF A PLACE

Street lighting has a strong effect on the character of a place. This includes both the distribution of the light, and the character of the light fixture/pole itself.



A mix of lighting types, including appropriate street lights, helps to establish the character of a place.

DO



Key Features

- A variety of lighting types should be used to address the different needs of civic spaces, pedestrian oriented streets, and parking areas.
- Lighting should be designed to provide a uniform distribution of light while also addressing safety needs.
- Lighting should be in keeping with the character of the spaces and buildings around them. Industrial poles and fixtures concerned purely with functional requirements should be avoided.

Street lights should reflect the character of the district, with articulated treatments for the base, post, and fixture.

AVOID



Designing site lighting purely to meet functional requirements denies its ability to support character of place.

LANDSCAPING

LANDSCAPING CONTRIBUTES TO A BEAUTIFUL PEDESTRIAN ENVIRONMENT AND HELPS ABSORB WATER.

Landscaping is critical to softening the urban environment. Arrangements should be both beautiful and resilient.



Landscape used as buffer between sidewalk and parked cars

DO



Key Features

- Select plants that are tolerant of drought, salt, and heat
- Species should be native where possible. No invasive species allowed.
- Use landscaping as part of stormwater management wherever possible.
- Use a variety of plants that will provide some color year-round.

Use plants that complement the scale of the architecture and street.



Integrate stormwater management into overall design.



Landscaping can help define the pedestrian realm.

Site Components

STREET TREES

STREET TREES PROVIDE SEVERAL PEDESTRIAN-FRIENDLY BENEFITS.

For pedestrians, street trees can serve as a buffer to vehicular flow and as protection from the elements, while also adding an element of natural beauty and softening an otherwise hard landscape.



Street trees can help delineate zones of a street and add character.

DO



Regularly-spaced street trees create a continuous canopy that covers sidewalks while physically and visually separating pedestrians and cars.



Where space is limited, trees can be incorporated in curb extensions.

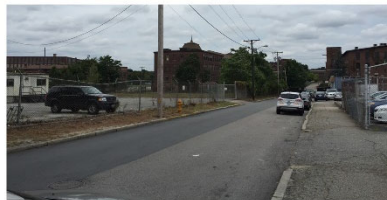
Key Features

- Continuous, regularly spaced street trees are best for creating a consistent canopy, but even periodic trees (where feasible) are worthwhile.
- Proper installation and maintenance are required to promote health and avoid hazardous situations.

AVOID



Improper maintenance or tree selection creates hazards.



Pedestrian-friendliness is hard to achieve without trees.

FENCES AND WALLS

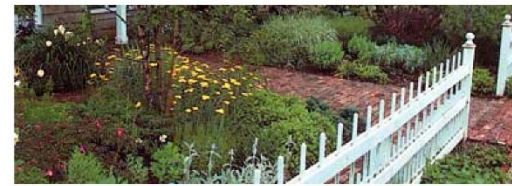
FENCES AND WALLS HELP HOLD AN EDGE WHERE BUILDINGS CAN NOT

Low stone walls and fences can mark the boundaries of a property to identify the edge of public and private space. They can also serve to help screen parking lots or define open spaces.



Fences and walls can have a big impact on the character of a place.

DO



Key Points

- Fences and walls can be used in combination with landscaping to help create separation between the public realm and private property.
- Construct fences of durable, paint-able materials such as wood, fiberglass, or wrought iron. Walls may be brick or stone masonry.
- The design of fences should be appropriate in scale and style to the building, site, and surrounding properties.



Dressed masonry walls, wrought iron, and decorative wood fences are appropriate in more urban and/or formal settings and can help define boundaries like parking edges or deep setbacks.

AVOID



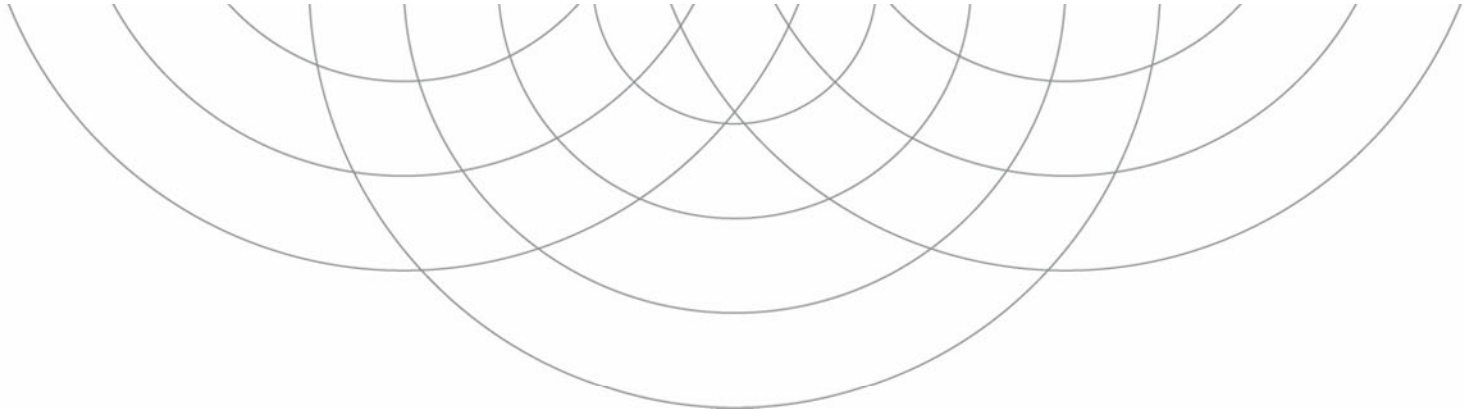
PVC, plastic and other synthetic fences have been shown to lower property values in historic neighborhoods.



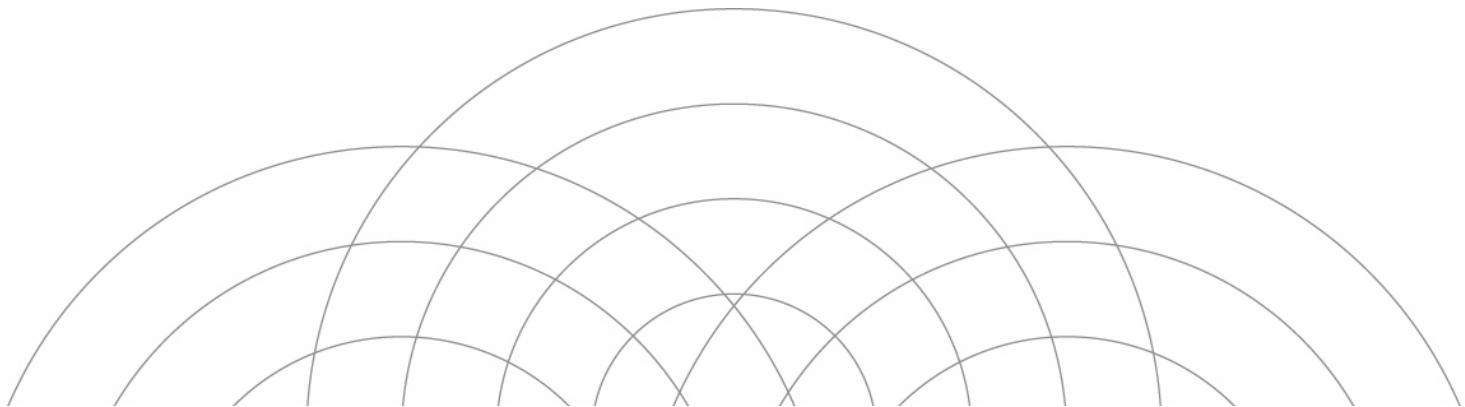
Chain-link fencing should be confined to the least visible locations.



Tall stockade type fences are typically not found in a mixed-use setting.



Illustrative Examples



Fort Hill Road from Newtown Road to Vergennes Ct



ILLUSTRATIVE EXAMPLES

POQUONNOCK BRIDGE VILLAGE

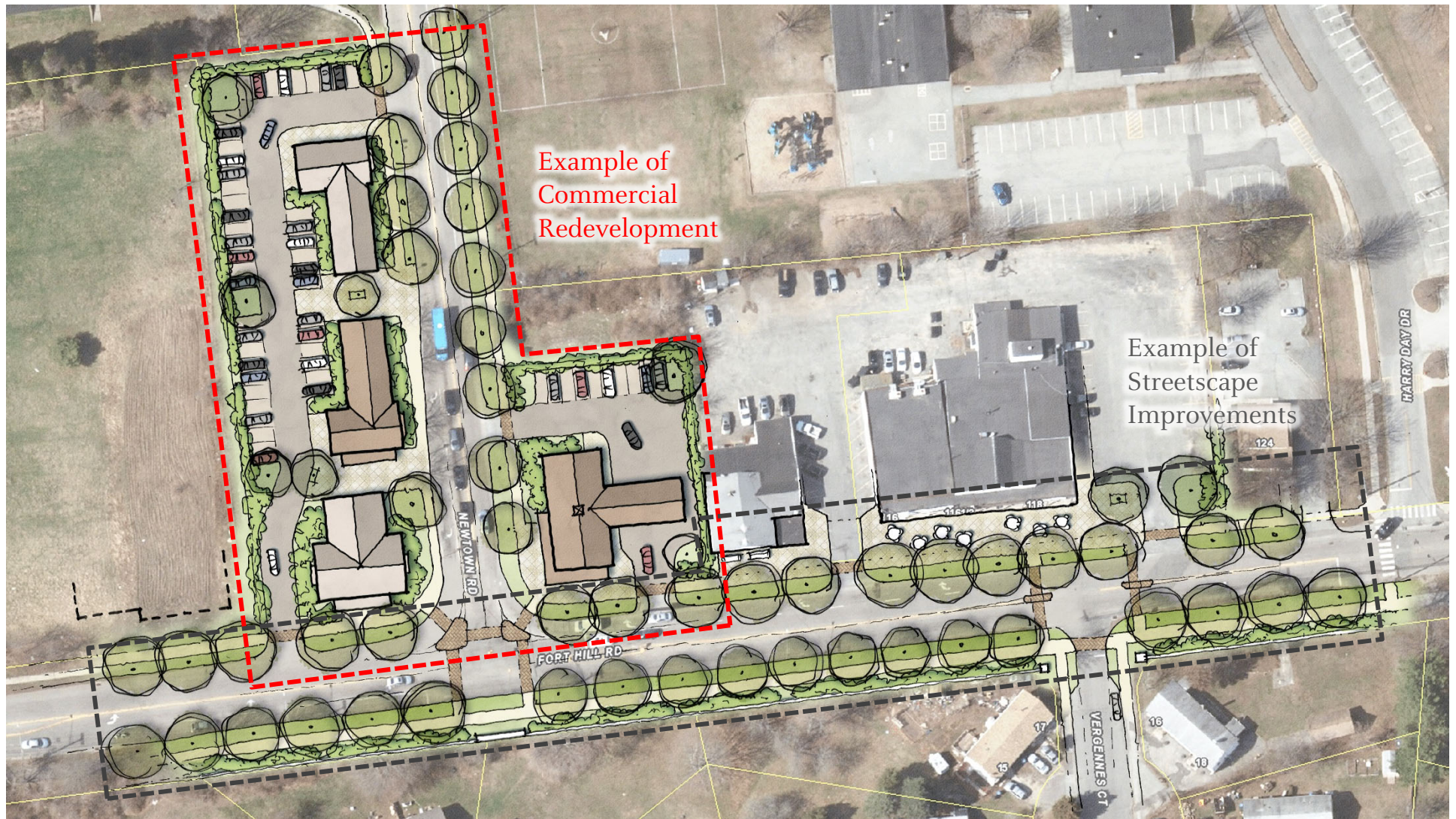
Fort Hill Road from Newtown Road to Vergennes Ct



ILLUSTRATIVE EXAMPLES

POQUONNOCK BRIDGE VILLAGE

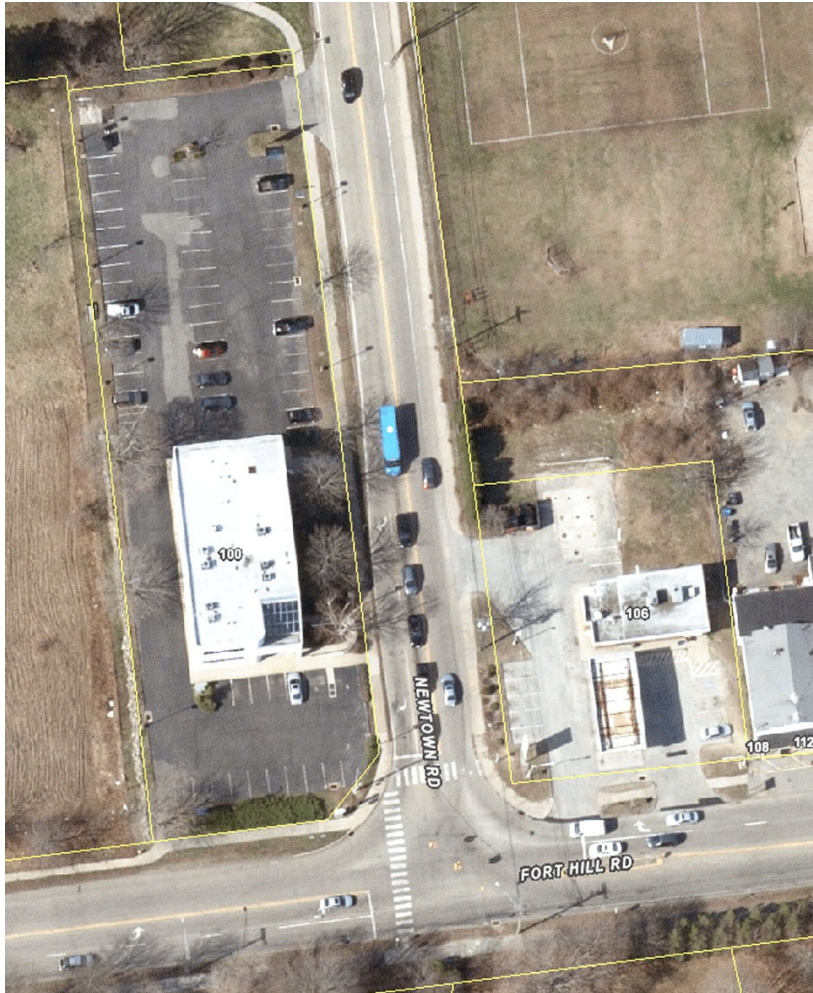
Fort Hill Road from Newtown Road to Vergennes Ct



ILLUSTRATIVE EXAMPLES

POQUONNOCK BRIDGE VILLAGE

Example of Commercial Redevelopment



New buildings fit village scale and character

Building entries face street

Buildings at the street with parking to side or back

Shared drives/parking areas

Screening for parking areas

ILLUSTRATIVE EXAMPLES

POQUONNOCK BRIDGE VILLAGE

Example of Streetscape Improvements



Improved
pedestrian
crossings

Cohesive screening
for residential
back yards

Parking located
behind/beside
structures

Building frontages
converted to
pedestrian uses

Continuous sidewalks behind
regularly spaced street trees

ILLUSTRATIVE EXAMPLES

POQUONNOCK BRIDGE VILLAGE

Example of Commercial Redevelopment and Streetscape Improvements



ILLUSTRATIVE EXAMPLES

POQUONNOCK BRIDGE VILLAGE

Example of Commercial Redevelopment and Streetscape Improvements



Continuous sidewalks behind
regularly spaced street trees

Building frontages converted
to pedestrian uses

Screening for
parking areas

Utilities relocated
underground

New Buildings and
entries face street

Improved pedestrian
crossings

ILLUSTRATIVE EXAMPLES

POQUONNOCK BRIDGE VILLAGE

Fort Hill Road at Depot Road/North Road



ILLUSTRATIVE EXAMPLES

POQUONNOCK BRIDGE VILLAGE

Fort Hill Road at Depot Road/North Road



ILLUSTRATIVE EXAMPLES

POQUONNOCK BRIDGE VILLAGE

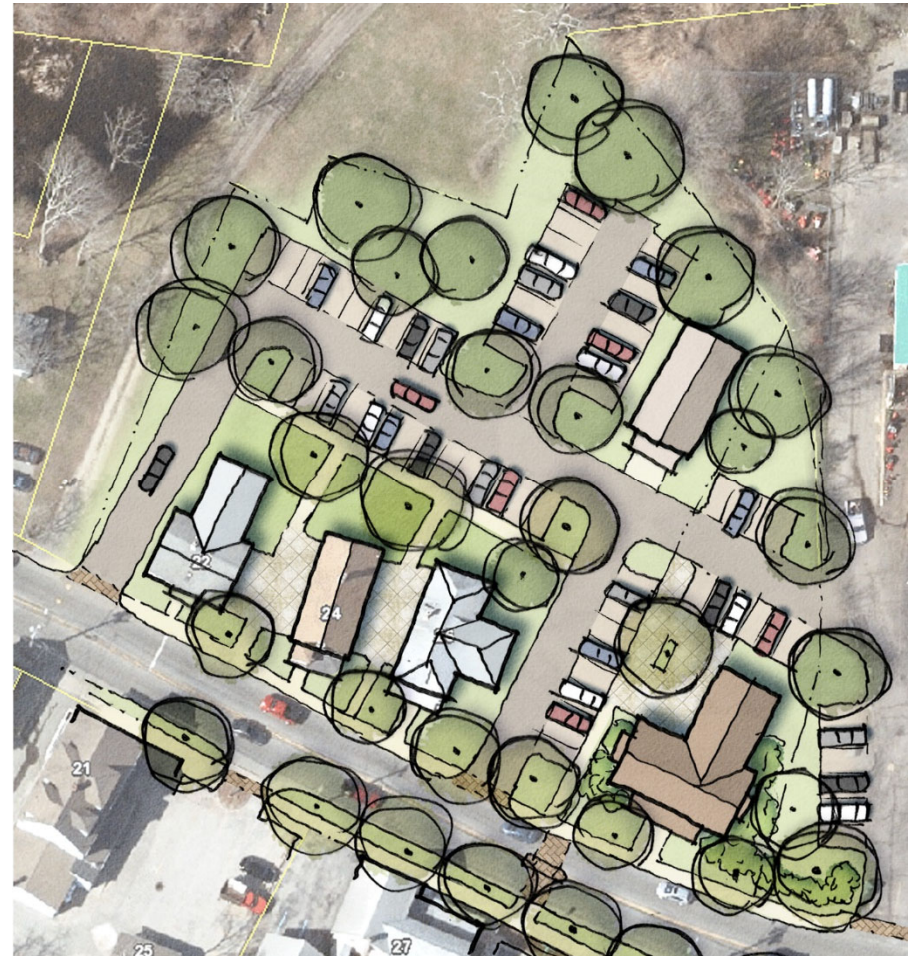
Fort Hill Road at Depot Road/North Road



ILLUSTRATIVE EXAMPLES

POQUONNOCK BRIDGE VILLAGE

Example of Historic Conversion



New additions secondary to
existing structures

New buildings fit village
scale and character

Building entries face street

Shared drives/parking areas

ILLUSTRATIVE EXAMPLES

POQUONNOCK BRIDGE VILLAGE

Example of Historic Conversion



ILLUSTRATIVE EXAMPLES



POQUONNOCK BRIDGE VILLAGE

Example of Infill Development



New buildings fit village
scale and character

Mixed use component at
street with residential behind

Design includes public
open spaces

Consolidated parking
areas and driveways

ILLUSTRATIVE EXAMPLES

POQUONNOCK BRIDGE VILLAGE

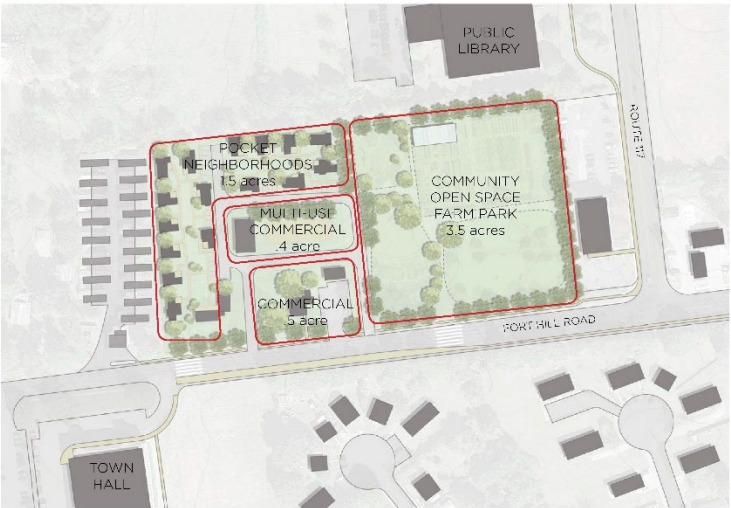
Example of Infill Development



1

Prepared for the Groton Family Farm LLC. Not for distribution

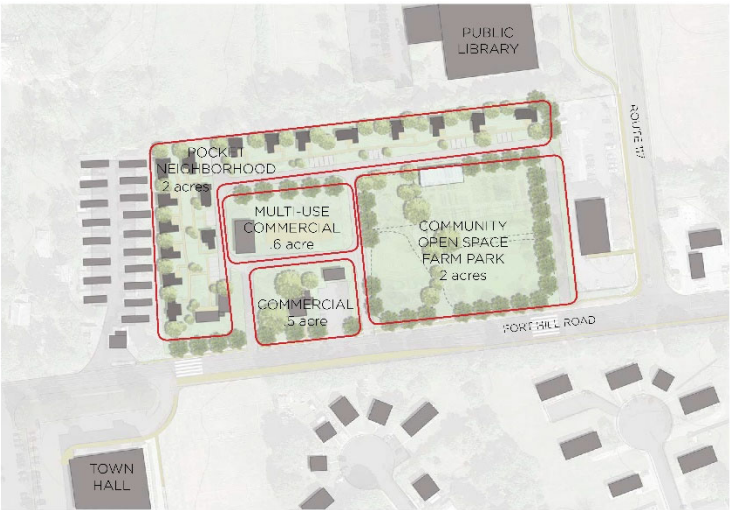
POSSIBLE SITE PLAN



12

Prepared for the Groton Family Farm LLC. Not for distribution

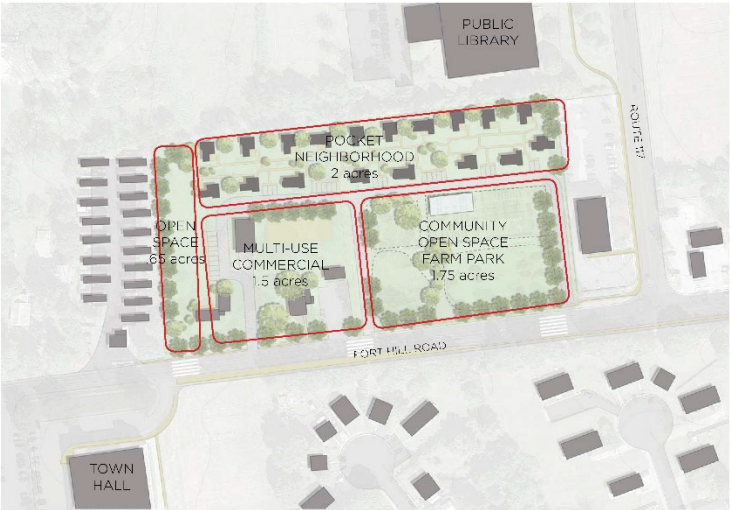
POSSIBLE SITE PLAN (ALT A)



18

Prepared for the Groton Family Farm LLC. Not for distribution

POSSIBLE SITE PLAN (ALT B) - LAND USE



21

Prepared for the Groton Family Farm LLC. Not for distribution

ILLUSTRATIVE EXAMPLES

POQUONNOCK BRIDGE VILLAGE

Example of Streetscape Improvements



Parking lot between Town Hall and Community Center converted to street

Continuous sidewalks behind regularly spaced street trees

Building frontages converted to pedestrian uses

Screening for parking areas

Improved pedestrian crossings

ILLUSTRATIVE EXAMPLES

POQUONNOCK BRIDGE VILLAGE

Example of Historic Conversion and Streetscape Improvements



ILLUSTRATIVE EXAMPLES

POQUONNOCK BRIDGE VILLAGE

Example of Historic Conversion and Streetscape Improvements



Continuous sidewalks behind
regularly spaced street trees

Building frontages converted
to pedestrian uses

Screening for
parking areas

Utilities relocated
underground

Building entries
face street

Storefronts allow
visibility into active uses

ILLUSTRATIVE EXAMPLES

POQUONNOCK BRIDGE VILLAGE

www.grotonCTzoning.com



Welcome to the Town of Groton's Zoning Regulations Update!

DISCUSSION

POQUONNOCK BRIDGE VILLAGE